



THE CHRISTIE HOSPITAL

STRATEGIC PLANNING FRAMEWORK

Rev 2 September 2014

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1 INTRODUCTION

- 1.1 This Strategic Planning Framework for The Christie has been prepared in partnership between The Christie NHS Foundation Trust ('the Trust') and Manchester City Council ('MCC').
- 1.2 The Christie has been pioneering cancer research breakthroughs for over 100 years and is the largest cancer centre in Europe. The facility treats more than 40,000 patients each year and was the first UK centre to be officially accredited as a comprehensive cancer centre. The Christie is a national specialist centre and treats 25% of cancer patients nationally. The main site is in Manchester (in Withington), with satellite radiotherapy centres in Oldham and Salford. The Trust employs around 2,400 staff and 300 volunteers (plus 300 University of Manchester staff employed at the Paterson Institute).



The Christie (Wilmslow Road entrance)

- 1.3 The Christie is a dynamic place that constantly needs to respond to the latest advances in research and treatment, often with new development. Within a surrounding residential context this future development needs to be appropriately planned in order to preserve

local character and to mitigate potential transport and parking impacts. The Christie also offers significant advantages to local residents, for example by providing employment opportunities and by supporting local businesses. The local community, through the Neighbourhood Forum, is an important partner for the Trust and is at the heart of this Framework.

- 1.4 The aim of this document is to provide an agreed framework for the ongoing development of the Withington site. It is intended to cover the next 15 years, i.e. the period 2013-2028. Obviously there will be more certainty about earlier years in this period. Future healthcare advances, and changes in treatment and funding at The Christie, that are not yet fully known are likely to influence the site in the later years of the period. The intention is that the document reflects the commitments and aspirations of the Trust for the next five years. Beyond this, the principles set out in the Framework will still apply but aspirations and potential development may need to be reviewed to reflect new healthcare progress. The intention is therefore to regularly monitor and review this Framework.
- 1.5 The following sections establish the importance of the Christie in the City context, showing how it contributes to the objectives of Manchester City Council, including those set out within the Core Strategy, and wider economic objectives. It identifies the significant projects that are known or envisaged and that will be essential components of the future development of this world class facility. The later sections provide a framework for future development that respects, and will not compromise, local character. Section 9 recommends measures to ensure that transport and parking elements will be fully addressed through the development strategy.
- 1.6 The Strategic Planning Framework has been the subject of public consultation and will be endorsed by Manchester City Council,

making it a material consideration against which subsequent planning applications will be assessed.

- 1.7 This follows an established approach by Manchester City Council to areas of change in the City and follows similar Planning Frameworks (e.g. for the Civic Quarter and First Street). The preparation of this Framework will bring more certainty to the site and its surroundings ensuring a more efficient planning determination process for future applications.

Figure 1.1 The Christie Site Boundary





2 STRATEGIC CONTEXT

Economic Context

Manchester Context

- 2.1 The Manchester City Region is a driving force in the regional and national economy. The City Region accounts for 52% of the North West's total economic output and 5% of UK output. Despite challenging economic conditions, the city region has significant strengths to build on, including:
- The second largest concentration of people and businesses in the country;
 - A highly skilled and relatively low cost workforce (compared to the south east);
 - An exceptional higher education offer;
 - A strong domestic business base;
 - A proven track record of attracting inward investment;
 - A strong and improving infrastructure offer that supports national and international connectivity.
- 2.2 The Manchester City Region continues to present opportunities for growth, diversification and productivity gains. There is a strong appetite for investment that can support the City Region to grow and deliver to its potential.

Role of the Health Sector

- 2.3 The health sector plays a central role in the Manchester economy. The Greater Manchester Growth Plan recognises the health sector's high growth characteristics, with employment growing by 13% between 2001 and 2011. Research by the New Economy (a commission established by the Association of Greater Manchester Authorities to create economic growth and prosperity for Manchester) found that the health sector is the second largest employer in the city region, employing over 150,000 people¹. Across the North West, the NHS spends £3.2bn on goods and services annually, of which 40% is retained in the region. More than half of spend in the North West is derived from Greater Manchester hospitals and PCTs.
- 2.4 In Manchester, the health sector employs 40,500 people (27% of the sub-regional total) and contributes over £1bn per annum to the economy. Institutions and businesses within the city are leaders in research and patient care at a national and European level. These include:
- **The Christie's** specialisms in cancer care, providing one of the largest cancer treatment centres in Europe;
 - The **Manchester Academic Health Science Centre** – the only one in the North of England – which is designed to support Greater Manchester to be a world leader in health research and education;
 - **Wellcome Trust's Clinical Research Facility** where over 375 studies have been approved since the facility opened in 2002;

¹ Business Register and Employment Survey (BRES, 2011)

- **The University of Manchester** whose Cancer Studies and Nursing Research were ranked first in the Research Assessment Exercise 2008;
- The presence of **Trustech**, the NHS body responsible for commercialising medical research; and
- Being the home of the **National Institute for Health and Clinical Excellence** (which sets drug standards worldwide).

2.5 This clustering of nationally and internationally significant organisations recognises the sector's importance within the local economy and also presents a strengthening asset on which to build. The mass of organisations supports the attraction of further organisations, facilities and expenditure into the City Region (see examples below) as well as highly skilled staff. This position continues to build Greater Manchester's reputation for high quality clinical care and research as well as providing opportunities for capture of wider economic benefits through greater levels of expenditure (by both organisations and employees) and supply chain linkages.

A Growing Sector in the Greater Manchester Economy

2.6 At a time of public sector budget cuts, there is an impetus to support continued innovation within the health sector. Steps are being taken to draw on the sector's assets, bringing together public and private sector expertise and attracting scarce investment to support growth and improved patient care. Significant opportunities are already being taken forward in the City including the development of:

- **Citylabs** – a new 100,000ft² bio-medical centre of excellence at the former Royal Eye Hospital, within the heart of Europe's

largest clinical and academic campus. The development (which is due to open in spring 2014) will provide wet laboratories, specialised research facilities, a hub for research and development and interaction plus teaching facilities for vocational training. The location of the facility in Manchester reflects the North West's membership of the 'European Super League' of biotech clusters by Strategem with Manchester providing the core of the cluster;

- **Manchester MediPark** – the MediPark is to be constructed across 200 acres around the University Hospital South Manchester. It forms part of the Manchester Enterprise Zone and will provide a nurturing and supportive environment in which businesses can grow. Links will be established between the hospital and companies located within the MediPark to support the development and commercialisation of drugs, medical devices and treatments. Occupiers are expected to include research and development functions, healthcare support services and commercial operations that complement core MediPark occupiers.
- **Circle Manchester** – Circle is Europe's largest employee co-owned partnership of healthcare professionals. In November 2011 Circle achieved full planning permission for a 10,500 sqm compact hospital on the Didsbury Point site in West Didsbury. Circle treats privately-insured and self-pay private patients, as well as government-sponsored NHS patients. Circle Manchester will admit patients for elective surgery and diagnostics, and is expected to have a throughput of around 600 patients a week.

The hospital will have the latest digital operating theatres and have the ability to carry out a full range of procedures including those that necessitate the need for an intensive care unit. This building was designed by architects Foster + Partners.

- 2.7 Continued growth of The Christie will further support the dominance of the health sector in Greater Manchester, through direct investment and building on the sub-region's growing reputation for excellence in both clinical care and research. Planned developments both at The Christie and across the City will contribute to job creation, economic growth and improved health outcomes.

Spatial Context

- 2.8 The Christie site is in Withington in South Manchester. It is located approximately 500m south of Withington District Centre. It lies within an established residential area with a number of local shops and small business lying nearby on Wilmslow Road. The site fronts both Wilmslow Road and Palatine Road. These are significant radial routes into the City Centre and are also major bus corridors. The site is also within easy reach of West Didsbury local centre (approximately 1 km south to the south-west via Palatine Road) and Didsbury Village district centre (approximately 1.5 km to the south via Wilmslow Road). Metrolink stations are now open in both these centres.
- 2.9 The main clinical site occupies land between Wilmslow Road and Palatine Road. Oak Road forms the southern boundary of the site, and is the location of the main entrance to the facility. The Christie buildings also present frontages to both Wilmslow Road and Palatine Road and Palatine Road has a major pedestrian entrance to the main corridor.

- 2.10 The Trust also occupies land on the eastern side of Wilmslow Road. This land, known as the 'Kinnaird Road site' lies between Cotton Lane, Kinnaird Road, Frith Road, Rathen Road and Cotton Hill. This site is used for supporting non-clinical uses, such as research, administration, nurses accommodation and car parking. The Wilmslow Road frontage of this site is the location for the prestigious new Manchester Cancer Research Centre (MCRC), which is due to open in late 2014.

The Need to Advance Cancer Care

- 2.11 Projections for the incidence of cancer in the UK show an increase in the number of cases to 2030. There is a need to cater for a growing patient demand and to innovate both in the service delivery models and treatments to ensure the best possible outcomes are achieved. The Christie sits at the forefront of this agenda.
- 2.12 The Department of Health's *Improving Outcomes: A Strategy for Cancer*, 2011 recognised the need to respond to patient priorities, including having timely and local access to assessments and the best treatment, and to be cost effective and efficient. The Christie is already responding to these priorities through recent and planned developments.
- 2.13 The Christie is already Europe's largest cancer centre and is the acute trust delivering the second highest number of research studies with a strong drive to continue to improve both clinical treatments and research advances, at The Christie and internationally. Continued investment, including in new and improved premises, is required to ensure that The Christie retains and builds on its leading position. This will be supported by other partners with strengths in the sector.

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- 2.14 The Trust and its partners continue to attract interest in high profile new research and treatment facilities at The Christie (for example the MCRC and Proton Beam Treatment Centre). These opportunities should be grasped and programmed in a way that ensures that the aspirations of the Trust can be met and wider social and economic benefits secured. If such new facilities cannot be located at The Christie then there is a significant risk that they will be developed at other locations and in association with other organisations outside of Manchester, with a resultant detrimental impact to the City's economy.

Figure 2.1. Wider Context Plan





3 LOCAL CONTEXT

Physical Context

- 3.1 The surrounding area is largely residential in character, with the exception of clusters of small businesses on Wilmslow Road. On Palatine Road there are many large Victorian houses which are now mostly in multiple occupancy or have been converted to separate flats. On other streets, particularly to the east of Wilmslow Road, houses largely remain as undivided residences for families.
- 3.2 The character of the area surrounding the site is noticeably different on each street that borders the site, with a difference in the status of the street and the Christie buildings' response to it. Each of these streets need to be considered in turn in order to understand the local character. The key defining characteristics for each street environment are summarised below.

Green Infrastructure

- 3.3 In 2011/12 Red Rose Forest worked with The Christie, Natural England and the Joint Health Unit at Manchester City Council on a Green Infrastructure plan for The Christie site in Withington, Manchester.
- 3.4 The plan has helped highlight and quantify the role that Green Infrastructure can play at a facility like The Christie.
- 3.5 Benefits include: reducing vulnerability to climate change, providing health benefits to patients, staff and visitors and cutting costs by improving building energy efficiency and reducing utilities costs. The plan involved detailed analysis of a range of different variables such as air quality, flood risk and noise pollution and provides a clear

picture of where new GI could be targeted to help address these issues.

- 3.6 This plan will continue to influence the development of The Christie campus and the street environment on the streets surrounding the site. The approach could be extended in partnership with Manchester City Council to improve legibility in the wider neighbourhood and direct visitors and staff from The Christie to the Metrolink stops and district centre.

Withington District Centre

- 3.7 The Christie is located immediately south of the Withington District Centre. In 2010 Manchester City Council published the 'Withington Village Action Plan 2010-2020' to inform decision making and guide future change. The vision for the centre is that "Withington will play a key role as a sustainable district centre that is an attractive place to live, work and shop." One of the aims of the Action Plan is that the Christie will be better linked to the district centre. This is to be done by promoting key walking routes between the site and the district centre (on Wilmslow Road and Palatine Road) and by improved signage.
- 3.8 The Trust recognises the important role that The Christie can have in supporting the district centre and support these initiatives. Additionally, increased employment numbers at the site will help to support local spending in the district centre.

Local Character Analysis

Wilmslow Road

- 3.9 Wilmslow Road was historically the main focus and primary frontage of The Christie. This is not the case now in functional terms but this remains the frontage where the facility is most often viewed by those passing by and is the point at which The Christie interacts most strongly with the Withington community. It is also the point where the main site connects with the Kinnaird Road site on the opposite side of Wilmslow Road. Although outside the district centre proper, there are clusters of local shops and small businesses alongside residential uses on Wilmslow and there are frequent double-decker bus services, which all create activity. There is also a pedestrian crossing here.



Wilmslow Road

- 3.10 The original 1930s buildings have since been much altered but the long curved sweep of this frontage, and retained 1930s elements (such as decorative boundary treatments and gates) present a strong

contribution to townscape, especially when viewed in the context of the Withington Green open space on the opposite side of Wilmslow Road. The new MCRC building will add a contemporary quality to the strong character of this setting. Local businesses and regular buses on Wilmslow Road create activity and a different functional character here to other streets. Views northwards to key buildings in the district centre (e.g. St Cuthbert's RC church, St Paul's church and the Red Lion PH) also create a strong local townscape.



Retained 1930s elements on the frontage

- 3.11 Contemporary development by the Trust has responded to this character and activity. For example, the recent addition of a glazed canopy at the pedestrian entrance, and the new MCRC building on the Kinnaird Road site.



The Golden Lion site (seen from the south)

- 3.12 The Golden Lion site lies a short distance to the south of the Christie on Wilmslow Road. The site, formerly occupied by a public house, has now been vacant for a number of years. The boundary has a low brick wall and benefits from several mature street trees on its Wilmslow Road frontage (which is now overgrown). The surrounding area is characterised by domestic scale inter-war and post-war housing and extensive vegetation to the street frontage within private gardens.
- 3.13 Key characteristics of the area comprise:
- Long continuous Christie frontages following the curve of the road.
 - Development of between two and three-and-a-half storeys.
 - Predominant use of brick.
 - Buildings set back from the street to varying degrees, often with no boundary treatment to allow increased space for public realm activity.

- In general, a wide road creates an open feel, with the curving road alignment allowing long views of the long Christie frontage.
- Buildings framing the triangular-shaped Withington Green, which creates a point of interest on Wilmslow Road.



Withington Green

Palatine Road

- 3.14 Palatine Road was opened in 1862 as a toll road, acting as a spur for development in the area. Today the long straight nature of the road reveals this history and the road remains an important traffic artery and bus corridor. Historically the status of the road led to a larger scale of development and the predominant established form here is of large two-and-a-half storey Victorian properties. These are set back from the street behind low stone walls with significant planting within front gardens. These large properties are now mostly subdivided into houses of multiple occupancy for young professionals or are sub-divided into flats. There has also been new development of significant scale, for example the large, four-storey Laurel Court

care home. Apart from The Christie, all buildings are in residential or institutional use, giving the area less pedestrian activity than Wilmslow Road and a different character.



Palatine Road

- 3.15 This form of development has been continued by the Christie buildings which front the eastern side of Palatine Road. The Wolfson Molecular Imaging Centre and multi-storey car park follow the established building line and present buildings of a significant massing, as does the proposed Young Oncology Unit (YOU). The building line actually shifts around 10m on the eastern side between no. 41 Palatine Road and the buildings to the north. This transition is resolved within the design of the YOU.
- 3.16 The property on Candleford Road (recently acquired by the Trust), sits on the opposite side of Palatine Road at the junction of Palatine Road and Candleford Road. This is a Victorian villa which has previously been converted to a public sector office use. It is two-to-three storeys with a steep roof pitch and several gables. The property is set back from Palatine Road behind a low stone wall and with extensive vegetation and mature trees on all sides. The building

addresses Candleford Road and there are areas of hardstanding on the other sides of the building. Candleford Road is a minor street that is now characterised by modern care home development, of three storeys, and extensive street trees and planting to the street frontage.



The Candleford Road property and its relationship with Palatine Road

- 3.17 Key characteristics of the area:
- A strong established building line (of around 8m and then 18m set-back from the back-of-pavement on the Christie side)/
 - Planting to the front of properties, including significant numbers of mature trees which creates a boulevard along the long straight road.
 - A consistent presence of low stone walls to define the front boundary of properties.
 - Buildings of significant massing, between two-and-a-half to four storeys with large footprints.

- The presence of both traditional Victorian materials (i.e. brick and slate) and contemporary materials (glazing, cladding panels, terracotta).

Oak Road

- 3.18 Oak Road, a secondary road linking the aforementioned two principal arteries, has a residential character and a quieter, more private character. The road forms an interface between the Christie and the surrounding residential community, with Christie buildings lying on the north side and residential properties on the southern side.
- 3.19 Existing non-Christie buildings are domestic in scale, ranging from two storeys to two-and-a-half storeys at either end of the road. Most properties are in terraces. These buildings are set back around 6m from the pavement edge with planting to the front boundary including a consistent row of mature trees at the street edge. The boundary treatment varies and includes low stone walls, hedgerows, fences and an open frontage.



Oak Road

- 3.20 On the northern side of the road the Christie buildings are of a significantly greater massing. Recent development and landscape enhancements has established this as the main entrance to The Christie. Buildings here are around four commercial storeys in height with large footprints, and are of a contemporary design. This development has established a building line with a set back from the pavement edge of around 16m. The landscaped entrance to the site occupies an area where buildings are set back further from the street.
- 3.21 The western part of the site is occupied by the soon to be vacated former Young Oncology Unit (which is being replaced by the new facility on Palatine Road) and by the Barry Court flats which lie outside the Christie site. In front of all these buildings is surface car parking, which represents the most accessible parking for patients.
- 3.22 Unifying this frontage is a strong landscape boundary to the street with a number of mature trees, similar to the landscape frontage on the opposite side of the road. The effect of this planting is to soften the streetscene, helping to integrate the larger Christie buildings and bring together the two sides of the street into a consistent character.
- 3.23 Key characteristics of Oak Road
- Christie buildings of around 4 commercial storeys with large footprints and a set-back of around 6m from the street.
 - Residential buildings between two and two-and-a-half storeys, in terraced form, with a set-back of around 5m from the street.
 - A strong landscape edge to the street on both sides with mature trees softening the streetscene.
 - Traditional materials on the southern side of the street and contemporary materials on the northern side of the street.

Tatton Grove

- 3.24 Tatton Grove represents a small but significant area of interface at the northern edge of the Christie site with its own distinct character. This is an area of surviving pre-industrial buildings with a smaller scale, more rural character. This is reflected in the inclusion of this area within the Withington Conservation Area. The most significant buildings in proximity to the Christie are the row of terraces that lie perpendicular to Tatton Grove and the Red Lion public house on the northern side of Tatton Grove.



Terrace on Tatton Grove

- 3.25 The terrace is interesting both because it lies at right angles to the street and because the properties have their own gardens that lie at the front of the dwellings, separated by a shared path (see photograph above). The Christie lies immediately to the south of this historic enclave. This has meant that a lower scale of Christie development has occurred in order to respect views from this part of the Conservation Area.

- 3.26 The Red Lion is a particularly historic building, dating from the 17th Century. It is Grade II listed. It also has its own bowling green, located behind a stone wall on Tatton Grove (the public house fronts Wilmslow Road). Although it is of a small height and not directly adjacent to the site this is an important building and its setting and the impact of the Christie on views from and to it need to be fully considered. It is also a well known local landmark building in this part of Manchester.
- 3.27 The key relevant characteristics of Tatton Grove are:
- Historic development of a more rural nature and more domestic scale.
 - The unique arrangement of properties and gardens associated with the terrace adjacent to the site.
 - The need for the Christie to respond to the sensitivity of views from the Conservation Area and the setting of the listed Red Lion.

Cotton Lane

- 3.28 Cotton Lane is a residential street that carries through traffic to other residential areas and to St Cuthbert's RC Primary School. The homes on this street are traditional two-storey semi-detached post-war houses.



Cotton Lane

- 3.29 The Kinnaird Road site of the Christie borders this road but does not present a built frontage to it. Instead there is a long brick wall, approximately 2m high, running the entire length of this frontage. This is reinforced by some significant mature trees lying behind the wall on the eastern part of the frontage. Access to the Christie site behind the wall is taken from a separate road that is accessed near the junction with Wilmslow Road. At the eastern end of the site there is an opening in the wall that provides access to the car park for the adjacent administration block.
- 3.30 Behind the wall, at the western end, is the site of the new MCRC (which at the time of writing is under construction). On the eastern part of the site are three three-storey brick blocks which are

principally used for nurses accommodation and offices. All of these buildings are set back from the road. The intervening mature trees and brick wall mean that they do not have a direct relationship with Cotton Lane and so the increased height of these buildings does not affect the streetscene of the domestic scale housing on the opposite side of Cotton Lane. The wall and mature trees provide a buffer which ensures that the Kinnaird Road site makes a positive contribution to the streetscene.

- 3.31 Key characteristics of Cotton Lane:

- The strong presence of the continuous brick wall in the streetscene.
- The positive contribution of mature trees to the streetscene.
- Traditional domestic scale post-war semi-detached houses.
- Larger scale Christie buildings set back from the street behind the wall.
- A dedicated separate access for Christie activities behind the brick wall.

Kinnaird Road/Frith Road/Rathen Road

- 3.32 This area has a similar character which is distinct from the other surrounding streets. It is a residential area with housing uniformly consisting of large late-Victorian semi-detached properties of two to two-and-a-half storeys in height. These are set back from the street approximately 6-8m in consistent building lines. The properties sit comfortably within spacious plots with fairly large back gardens. The street frontages consist of low brick walls (approximately 1m high) with stone gate piers. Significant planting behind these walls serves to soften the streetscene.

3.33 This housing has a changing relationship with the Kinnaird Road site along its southern boundary. Houses face onto Kinnaird Road, opposite the site. On Rathen Road they back onto the site at its eastern end. Frith Road has no properties fronting it, instead the side elevations and boundary treatments of properties on the other two streets face this road.

3.34 There is little built development on the Kinnaird Road site to any of these frontages, a previous building having been recently demolished. The exception is a series of single storey brick buildings adjacent to Kinnaird Road, which currently house Trust administrative functions. These do not play a significant role in the character of the street. Most of the Kinnaird Road site in this area is currently used for car parking, although the western part of the Kinnaird Road frontage will be extensively landscaped as part of the MCRC proposals.



Kinnaird Road

3.35 The Kinnaird Road site presents a similar boundary treatment to these streets. This consists of a low brick wall (generally approximately 1m high) behind which there is considerable planting.

A similar treatment also exists to the rear of the properties on Rathen Road. On Kinnaird Road a boulevard of mature trees on the Kinnaird Road frontage makes a very strong contribution to the street scene.



Frith Road



Rear of properties on Rathen Road

3.36 The area has the following characteristics:

- Two-and-a-half storey late Victorian large semi-detached houses within spacious plots.
- A frontage on both sides of streets consisting of 1m high brick walls with planting behind.
- A strong boulevard of trees on the Christie's Kinnaird Road frontage.
- A changing relationship between the Kinnaird Road site and fronts/backs of adjacent housing.

Cotton Hill

3.37 Cotton Hill is a residential street accessed off Cotton Lane. It lies at the eastern end of the Kinnaird Road site. The site has no direct relationship with this street but properties on the western side of the street back onto the site. These properties include some recently semi-detached housing at the northern end of the street (on the site of the former Cotton Tree public house), to the south of this a Victorian terrace and further south a post-war terrace. All of these properties are two-storey and have short back gardens.

3.38 The Kinnaird Road site behind these properties is currently used for car parking, with two linked administration buildings located in the north-eastern corner of the site. The site responds to this boundary with a brick wall with intermittent planting behind. This planting is significant at the southern part of the site and behind the new housing but thins out behind the Victorian terrace. The wall is around 1m on the Rathen Road side of the site. On the Cotton Hill side the wall is generally around 1.5m tall but rises northwards, to a height of around 3m adjacent to the administrative buildings. There is a change in level along this boundary. The site is approximately level with its surroundings at the southern end but in the region of the

administrative buildings the site ground level sits approximately 0.5m lower than the housing to the east (a sudden drop behind the tall brick wall). To the south of these buildings the level change is evident as an earthwork mound along the eastern edge of the site.



Site relationship with Rathen Road properties

Figure Ground Analysis



Aerial Photograph of Local Context



The Christie - Strategic Planning Framework

Figure 3.1: Site Context Analysis



4 STRATEGIC FIT

Economic Context and Benefits

- 4.1 The role of the Trust within the Manchester economy is already significant with future development plans to further contribute towards local and national economic priorities. The table below summarises the contributions The Christie's development proposals will make to both national and sub-regional policy priorities.

Table 4.1: Anticipated Contributions

Strategy	Anticipated Contribution
Manchester Independent Economic Review	Plans to contribute towards continued growth within the City Region, as a driving force for the national economy. Expansion of The Christie will build on the identified cluster of major health services, research and teaching facilities already found in the city region. Productivity gains will be supported by investment in physical capital and innovation identified through research and later applied in the service offer.
Prosperity for All: The Greater Manchester Strategy (AGMA)	Developments will continue the exploitation of Manchester's world class knowledge base and support the improved health of residents. By continuing to build on research and healthcare strengths, investments will allow for the employment of highly skilled workers within the City Region, including by helping to attract the

	most talented people and organisations.
Greater Manchester Growth Plan (Greater Manchester Economic Advisory Panel)	The strategy recognises health as a high growth sector which the Christie's ongoing development will continue to support. Potential to attract a growing healthcare business base into the sub-region to build on the identified global commercial potential of the sector.
Greater Manchester LEP Proposal to Government (Greater Manchester LEP)	Opportunity to continue to build on world class research, science and innovation capabilities which is identified as an economic priority. Development also provides an opportunity to lever further investment into the sub-region and to grow the bio-medical sub-sector (an identified priority sector).
South Manchester Strategic Regeneration Framework (SRF) (Manchester City Council)	The SRF recognises the Christie's existing role as a major employer and the need to support its planned expansion. Phased development will support the SRF's second policy theme of achieving full potential in education, skills and employment by supporting the growth of local employment opportunities.
Strategy for UK Life Sciences (Department for Business Innovation and Skills)	Development at The Christie will support continued innovation and the advancement and commercialisation of research as a priority. In return, the Trust will benefit from the reduced bureaucracy offered by Government in relation to clinical trials and steps to continue to attract and nurture talent. This will allow an expanding programme of activities to be accommodated on site and for research advances to be translated into clinical practice more quickly.
Innovation and	This strategy recognises the need to innovate to

Research Strategy for Growth (Department for Business Innovation and Skills)	respond to both challenges and opportunities and identified life sciences as a key technology based sector within the UK. Expansion of The Christie will support a larger research programme that can be translated into clinical solutions, including through collaboration with academic institutions. Significant Government contributions will support development plans, e.g. investment in the Proton Therapy Centre.
Science and Innovation Investment Framework 2004-2014 (HM Treasury)	Ongoing development of The Christie complex supports the framework's drive to support a continued ability to compete on high technology and intellectual strength, helping to make Britain the most attractive place in the world for science and innovation. The importance of research and innovation within the health sector is emphasised through the framework. Investments at The Christie will build on existing strengths and explore new areas of innovation.
Innovation Health and Wealth; Accelerating Adoption and Diffusion in the NHS (Department of Health)	The need for innovation to support the future operation of the NHS is highlighted, both through research and new delivery approaches. Ongoing investment in The Christie site will contribute to both of these agendas. It will provide new and innovative facilities and service offers for patients and advance the research process to support ongoing innovation within the Trust's operations and more widely across cancer care. The co-location of research and clinical care on a single site will accelerate the transition from research to practice.

Planning Policy

- 4.2 The Strategic Planning Framework is a planning document that forms the part of the planning policy context for the site. It should be read alongside the National Planning Policy Framework (NPPF), the Core Strategy for Manchester and the Saved Policies that form part of the Local Development Framework. As the summary below shows, this Strategic Planning Framework is fully consistent with this planning context and will help to deliver the objectives of both the NPPF and the Core Strategy. Any future development on the site must also comply with all other relevant national and local planning policies.

National Planning Policy Framework

- 4.3 The **National Planning Policy Framework** (NPPF), published on the 27th March 2012 by the DCLG, sets out the overarching planning policy framework in a single, coherent document.
- 4.4 The NPPF states that "At the heart of the planning system is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan making and decision-taking". Local planning authorities should positively seek opportunities to meet the development needs of their area and approve development that accords with the development plan without delay. Sustainable development is defined as development that meets the needs of the present without compromising the ability of future generations to meet their own needs. It has economic, social and environmental dimensions [NPPF, p2-4].
- 4.5 It identifies a Core Planning Principle for the need to "take account of and support local strategies to improve health, social and cultural wellbeing for all, and deliver sufficient community and cultural facilities and services to meet local needs" [NPPF,p6].

- 4.6 The Government attaches great importance to the design of the built environment. Developments should add to the quality of an area and establish a strong sense of place, responding to local character and history whilst not preventing innovation. Safe, accessible and visually attractive developments should be created that optimise the potential of the site to accommodate development and support local facilities and transport networks [NPPF, p15].
- 4.7 The NPPF promotes sustainable transport. Developments that generate significant movement should be located where the need to travel will be minimised and the use of sustainable modes can be maximised. Plans and decisions should ensure that the opportunities for sustainable modes have been taken up and that safe and suitable access can be achieved for all people. A key tool in facilitating these measures will be the Travel Plan.

Core Strategy for Manchester

- 4.8 The **Core Strategy** for Manchester was adopted on 11 July 2012 and now forms the primary development plan that covers the Christie site. The 'spatial priorities' for the Core Strategy are set out in Policy SP 1. These principles will guide the strategic development of Manchester to 2027. It includes the following 'Core Development Principles':

Development in all parts of the City should:

- *Make a positive contribution to neighbourhoods of choice;*
- *Minimise emissions, ensure efficient use of natural resources and reuse previously developed land wherever possible;*
- *Improve access to jobs, services, education and open space by being located to reduce the need to travel and provide good access to sustainable transport provision.*

- 4.9 The Christie, and its associated future development aspirations, makes a positive contribution to these objectives by making effective use of an existing employment site, providing local jobs in a sustainable transport location and supporting the local housing market and district centres.

- 4.10 The Christie site is within the South Manchester area, covered by Policy EC9:

Policy EC 9: South Manchester

South Manchester is not expected to make a significant contribution to employment provision within the City. New development is expected to mainly comprise office development, although proposals for high technology industry and research will also be supported. Employment and economic development provision will be within:

*1. Existing employment locations, such as Business Parks -i.e. Towers, Didsbury Point, Christie Fields and Parkway; **Christies Hospital**; Along Princess Parkway;*

2. District Centres of Chorlton, Didsbury, Fallowfield, Levenshulme and Withington. These will be suitable for mixed use development providing local needs and services.

Development proposals and planning applications should have regard to:

- *Ensuring efficient use of existing employment space;*
- *Improving public transport, walking and cycling connectivity between residential neighbourhoods and employment locations such as the City Centre, district centres, Trafford Park and Manchester Airport, particularly orbital connectivity.*

- 4.11 This policy therefore supports further economic development at The Christie site. It will be important to make efficient use of the space

(which is a priority given the constrained nature of the site). The Transport and Parking Framework responds to the policy above.

- 4.12 There are a number of important **environmental policies** which the development has had regard to and which will further influence design development at reserved matters stage. These policies (EN4 to EN8) set out the City's aspiration for development to contribute to high standards of sustainability. The proposed development already achieves many of these aims by being located in a brownfield, urban location that is highly accessible by public transport (i.e. accessible by frequent bus services on Wilmslow Road and Palatine Road and within walking distance of the Metrolink).
- 4.13 The Development Framework will make a positive contribution to these policies. The site is well located, in being easily accessible by public transport and the framework responds to design and sustainability policies.

Saved Policies

- 4.14 Many **UDP** policies also remain extant. This includes site specific policies for small areas that affect the Christie site.
- 4.15 The site falls within the **Withington and Burnage** sub-area for which the general policy (Policy **WB1**) is to:
- ensure that the area remains an important location in which to live and work;
 - protect and enhance the quality of Withington village and other shopping areas as local shopping centres;
 - retain the primarily residential character of the area;
 - encourage commercial opportunities only where they will not cause harm to residential amenity and where they will have clear job creation advantages;

- promote the use and exploit the potential of recreational facilities, particularly open spaces, for leisure activities.

- 4.16 The Christie is specifically identified with reference to policy **WB2**:
- In considering proposals for the expansion and/or redevelopment of major employment sites in the area, particularly the **Christie Hospital** on Wilmslow Road, the Council will have regard to the need to minimise any impact upon the environmental quality and character of the area, residential amenity, and traffic movements.*
- 4.17 This policy clearly supports expansion of The Christie, whose importance is recognised. The requirement to consider environmental quality, residential amenity and traffic movement echoes the driving principles of this Framework and the analysis that has informed the Development Framework and Transport and Parking Framework .

Guide to Development in Manchester

- 4.18 The City Council has published a revised version of its design guide: 'The Guide to Development in Manchester' (adopted in 2007). This is a Supplementary Planning Document against which all planning applications will be assessed. The Guide primarily deals with the design of buildings, streets and spaces and also mix of uses and density of development sites. It is acknowledged as a source of good advice in ensuring best practice and quality urban design. The following principles are addressed by the Guide:
- Design
 - Accessibility
 - Environmental Standards
 - Street Hierarchy
 - Parking Guidelines

-
- Housing Density
 - Promotion of a Safer Environment
 - Design for Health
 - Internal Design Principles

4.19 The Development Framework and Design Principles within this document follow the guidance set out in the Guide for Development in Manchester. The Guide should also be referred to when designing the proposed development on the site.



5 THE CHRISTIE TODAY

Christie Activity

5.1 The Christie treats more than 40,000 patients each year and was the first UK centre to be officially accredited as a comprehensive cancer centre. The Christie is recognised as a national specialist centre and is the leading provider of cancer treatment in the North-West. It caters for 270,000 patient visits every year of which 240,000 visits are to the Withington site. The scale of the Trust's activities makes the Christie a major employer. The Trust employs approximately 2,400 staff.

5.2 The **'Mission'** of the Trust, as set out in its Commercial Strategy, 2010 is:

"Our mission is to be a world leading cancer institute. To achieve our mission we must retain The Christie as an independent organisation which is demonstrably well managed, meets nationally determined objectives, has secure inter-organisational partnership arrangements and is internationally recognised as one of the world's leading comprehensive cancer centres."

5.3 The Trust has the following **'Core Principles'**:

Our mission and strategic objectives are supported by nine core principles. We will:

- *Provide an excellent experience for everyone who comes into contact with us;*
- *Maintain our independence and autonomy as a specialist comprehensive cancer centre;*

- *Lead the provider network of cancer services to ensure high standards of cancer care research and education are available throughout the north west of England;*
- *Be one of the UK's leading health care employers as recognised by our staff;*
- *Adhere to the core set of Christie values;*
- *Be compliant with all national standards for performance, quality and financial management;*
- *Foster effective relationships with external stakeholders;*
- *Demonstrate value for money, efficiency and effectiveness in everything we do;*
- *Protect the environment and enhance our local community.*

5.4 The Christie is already a pioneer in radiotherapy advances, for example, delivering the first clinical use of multileaf collimation in the UK, the first clinical use of intensity modulated radiotherapy (IMRT) in the UK and the first clinical use of image guidance radiotherapy (IGRT) worldwide. This reputation will continue to be built upon through new development and continued investment in existing facilities.

5.5 The co-location of clinical services and research facilities on a single site is a significant strength. It allows basic science and clinical expertise to be integrated, allowing developments in experimental medicine to be aligned with clinical need to ensure rapid translation into practice.

The Christie's 20:20 Vision

- 5.6 The Trust published its '20:20 Vision' in September 2012. This is new strategy, up to the year 2020, and derives from a process that included a series of public engagement and stakeholder events, staff consultation and engagement with key partners. Four key themes were identified, each with ambitions for the future:
- 5.7 Leading cancer care:
- To be recognised by healthcare professionals and the public as one of the world's top five cancer centres.
 - To be the hospital of choice for patients requiring specialist cancer treatment across the country.
 - To demonstrate excellent clinical outcomes and continued improvements in survival rates.
- 5.8 The Christie experience:
- To employ the best clinical staff.
 - To extend services into local communities, with more flexible options for cancer follow-up care.
 - To develop a new Sir Norman Foster designed Maggie's Centre to provide holistic support services for patients.
- 5.9 Local and specialist cancer care:
- To develop more chemotherapy closer to patients' homes using a mobile unit.
 - To extend the network radiotherapy centres.
 - To reduce delays in diagnosis and provide direct access to specialist services and offer patients more choice.
 - To tailor services to local needs.

- To improve access to our specialist cancer surgeons.
- To be recognised internationally as a specialist centre for minimally invasive cancer treatments.

5.10 Best outcomes:

- To make surgical care available to more patients and further develop expertise in bowel, urological, gynaecological and plastic reconstructive surgeries.
- To provide world-class radiotherapy research, building on the existing programme and the proton beam service.
- To achieve national recognition by award of Biomedical Research Centre Status.

- 5.11 Clearly a broad range of actions will be required to deliver these aspirations, and developments at the main Withington site will play a fundamental role in meeting many of these objectives.

Economic Contribution

- 5.12 The Christie has an important presence in Withington, the City and the North West. In addition to the high number of patient visits the scale of the Trust's activities makes the Christie a major employer. At the time of writing the Trust currently employs 2,394 people across 2,173 full time equivalent posts (excluding around 300 volunteers and 300 University staff at the Paterson Institute). It makes an important contribution to local employment – approximately 21% of the workforce lives in the M14, M20 and M21 post codes that surround the Withington site, 38% in Manchester as a whole and 49% elsewhere in Greater Manchester.

- 5.13 In 2011/12, the Trust's wage bill was £77.374m (excluding National Insurance and pensions). This equates to an average wage of £35,600 per full time employee, making The Christie a high value employment provider². A wide range of posts are supported, including medical and research posts as well as in-house catering and estates maintenance roles, providing employment opportunities for people with a cross-section of skills and experience.
- 5.14 Expenditure on other goods and services totalled £65.5m in 2011/12. This is a substantial investment and a high proportion is captured within the City and across the sub-region. Of the £65.5m spent, 19% (£12.1m) was spent in Manchester and 11% (£7.3m) elsewhere in Greater Manchester. Spend within Greater Manchester is estimated to support approximately 170 jobs in the sub-regional economy³.

Partnerships

- 5.15 The Christie is working with partners to support Manchester to exploit its strengths in health research and innovation. The Trust regularly engages local, national and international partners who are leaders in their field. Partners include the University of Manchester, University of Liverpool, the Manchester Academic Health Science Centre and Cancer Research UK. Collectively, the organisations are ensuring that Manchester's research and clinical strengths continue to be built upon at both a national and international level to advance innovation in cancer treatments and success rates.
- 5.16 The Christie is also committed to working with the local community. For example, the Trust:

² Annual Survey of Hours and Earnings, ONS, 2011 shows full time earnings in Manchester average £25,900.

³ Across the North West economy, expenditure of £113,701 is required to support one full time equivalent job.

- Offers medical careers days to 120 sixth form students per year and 60 linked work experience placements;
- Provides volunteering opportunities, resulting in over 300 volunteers currently being on the database;
- Works with local employers and community groups to understand local issues and needs.

- 5.17 The opening of the new education centre will allow further events to be offered to local residents to allow them to understand more about The Christie's activities. An expanded Apprenticeships programme will also be offered to allow people to gain practical experience of working in the health sector.

Local Employment and Suppliers

- 5.18 The Christie is committed to securing local benefits through its activities. Local employment and procurement policies are in place and will apply across future phases of development. Current employment and expenditure patterns (outlined earlier in this report) demonstrate the pro-active measures being taken by The Christie to capture a high share of employment and expenditure locally. Diversity and competition is supported through the Trust's Commercial Strategy, including the engagement of SMEs and the voluntary and community sector, whilst local businesses are encouraged to bid for supply opportunities through the publication of requirements and simplification of procedures.

Local Employment Partnerships

- 5.19 Local Employment Partnerships are a deal between Government and employers to tackle the increasing recruitment and skills challenges of today's labour market and economy. The aim is to help organisations find the right people, and people in the right numbers by accessing the range of Government services available to employers. They are designed to build on organisations existing approaches to recruitment by bringing new opportunities and services.
- 5.20 There are many people with potential who organisations cannot reach by either traditional recruitment methods like newspaper advertising or with electronic media like online job boards and internet job sites. This is where Local Employment Partnerships can assist and connect us to these people from within the local community. The scheme can provide people with an opportunity to build confidence in applying for a new job and expand the organisations labour market giving access to people in the local area.
- 5.21 Job centre plus have nominated a named contact to manage the agreement with the Christie NHS Trust at a national level. Local advisers will get to understand The Christie's business and recruitment needs. The advisers will help to fill vacancies at The Christie using their knowledge of the local labour markets, finding and funding the right skills support for the trust drawing on their network of skills and training partners through to making sure the vacancies are open to the talent available and effectively publicised in the best places.
- 5.22 The Trust has a Local Employment Partnership Agreement that includes commitments to:
- Review recruitment processes to ensure that they do not unnecessarily exclude anyone;
 - Give current employees the chance to represent the Christie and mentor people who want to return to work;
 - Try new ways to help people take up a job for example offering flexible working patterns;
 - Offer funded work trials, for an agreed number of potential recruits;
 - Hold group seminars, run by Jobcentre Plus advisers to introduce potential recruits to work; and
 - Work with Jobcentre Plus and partners on the design of pre-employment training, where required, for potential recruits to ensure that it is relevant to employer's needs.
- 5.23 There are a number of other initiatives that the Trust will consider in the future as part of the Local Employment Partnership agreement which include joining forces with the job centre to develop pre-employment training and offering opportunities for people who complete the training and working with the Skills Academy for Health with their pre employment programmes.
- 5.24 The Trust is also committed to increase the proportion of locally employed staff.
-

Recent Development

- 5.25 The Trust has been active in recent years in securing investment and is currently in the process of delivering some significant new committed development projects. These '**committed**' projects are considered to be schemes that are currently in the process of being delivered. Figure 5.1 describes the existing Christie site, although this is due to change when these committed projects are implemented.
- 5.26 The **Manchester Cancer Research Centre** (MCRC) – a joint project with the University of Manchester this major new research centre will support the expansion of research into personalised cancer treatments. This is located on the Kinnaird Road site and is due to open in late 2014.
- 5.27 A new joint **YOU/HTU** facility on Palatine Road has recently been constructed. The YOU (Young Oncology Unit) replaces outdated accommodation on Oak Road and will provide new specialised accommodation for teenagers with cancer. The HTU (Haematology Transfer Unit) provides enhanced clinical accommodation for treatment of blood cancers and provides a co-locational benefit for teenagers with blood cancers.
- 5.28 In order to mitigate car parking spaces lost to the MCRC development a re-configuration of surface car parking spaces on the Kinnaird Road site has recently been implemented.

Transport and Parking Overview

- 5.29 The Christie is a major generator of travel in south Manchester. There are two distinct groups of travellers to the site, employees and patients. There are some 240,000 patient visits to the site each year. The majority of these are outpatient visits; there are only 170

inpatient bed spaces in the Christie. On the busiest days there are just under 1,000 patients treated at the site.

- 5.30 There are about 2,700 people employed on the site, including just over 300 employed by the University of Manchester in the Paterson Building. As with all similar healthcare facilities, there is a wide range of types of employment but because of the predominance of outpatient care the majority at the Christie work the "day shift" albeit with many different hours worked.
- 5.31 The site is located on the Wilmslow Road corridor which is renowned for its high frequency bus services. To the north of the site in Withington the Wilmslow Road corridor effectively splits with one section, Palatine Road, heading to West Didsbury and Northenden and the other, the continuation of Wilmslow Road, heading to East Didsbury and Cheadle.
- 5.32 Palatine Road forms the western boundary of the site while Wilmslow Road runs through the site. The majority of activity including all patient care takes place on the section of the site between Palatine Road and Wilmslow Road. This section of the site is commonly known as the Palatine Road side.
- 5.33 There are research and administrative activities on the section of the site to the east of Wilmslow Road. This section of the site is commonly known as the Kinnaird Road side – the main access to it is from Cotton Lane – and is where future research facilities will be developed such as the MCRC building.
- 5.34 The area surrounding the Christie is primarily residential in nature. Many people live in walking and cycling distance of the site.
- 5.35 This high level of accessibility means that many people travel to work by sustainable modes of transport and this number has increased in recent years. This has been supported by the Trust's Travel Plan which has been in place since 2005 and has been given the Green

Apple Award for environmental best practice. Regular surveys of staff travel to the site are undertaken the results of which are discussed in Section 7, but in summary the surveys of journeys to work in 2012 showed:

- 11% using public transport
- 6% cycling
- 15% walking
- 5% car sharing and
- 63% car driving

5.36 Notwithstanding this, it is travel to the site by car which predominates and it is issues around travel by car, particularly car parking which are the most recognised transport concerns associated with the site. The Christie is of course not alone in this. It is a characteristic shared by the majority of large healthcare sites.

5.37 Surveys of the patient/visitor parking (in the patient/visitor car parks) taking place on site were undertaken in November 2012 (5th to 16th November). These showed that the patient/visitor car parks are effectively full on a daily basis.

5.38 At the time this report was first compiled there were about 338 patient/visitor spaces and 510 staff spaces of which 460 were in the two main staff car parks (Kinnaird Road and the multi-storey car park on Palatine Road). The surveys show that at the busiest time of the day (which is late morning) effectively all the surveyed spaces were full. If it is assumed that the small number of un-surveyed spaces had a similar level of occupancy then the level of parking currently taking place on site is about 800 cars of which about 330 are patients/visitors and 470 are staff.

5.39 Comprehensive surveys of on- street parking were also undertaken in November 2012 (26th to 29th November 2012) over the area shown in Figure 5.2. The surveys were undertaken once at night to identify residents' cars and then repeated every two hours during the working day to identify the numbers of non-residents parking in the survey area. The survey was designed to differentiate between short stay parkers (likely to be patients) and longer stay (likely to be staff). A summary of the results is shown in the table below.

Table 5.1: Parking Survey Results

	09:00		11:00		13:00		15:00	
	Resident	Visitor	Resident	Visitor	Resident	Visitor	Resident	Visitor
Monday	249	858	213	884	195	868	168	757
Tuesday	206	873	176	947	167	913	163	830
Wed'day	195	906	152	934	138	930	128	897
Thursday	193	860	172	924	169	913	163	876

- 5.40 There were up to some 950 non-residents cars parked in the survey area, the majority of which it is considered will be associated with The Christie. Of these 820 were longer stay and are assumed to be staff while 130 were short stay.
- 5.41 A summary of all these surveys is included in Appendix 1.
- 5.42 The Trust is funding the implementation of a parking scheme on streets surrounding the Christie. Manchester City Council is currently preparing the scheme, but the concept is to apply a time limited waiting restriction which would allow short stay parking but prevent longer stay. This would allow patients to park on street but prevent staff from doing so. Local residents would be exempt from the long stay ban if they have a permit. The extent over which the scheme is being consulted on is shown in Figure 5.2.
- 5.43 The consultants preparing this study have spoken to the City Council about the scheme but made no formal representations to them. There appear to be a number of locations within the proposed scheme area where longer stay on street parking could be accommodated without a detrimental impact on local residents. The south side of Cotton Lane and on Parkville Road alongside Fog Lane Park would appear to be the most obvious examples.

Servicing

- 5.44 There are a number of points from which the site is serviced on Palatine Road, Wilmslow Road, Oak Road, and Kinnaird Road. There are no proposals to change this servicing arrangement going forward.

Figure 5.1: The Existing Christie Site (main site)

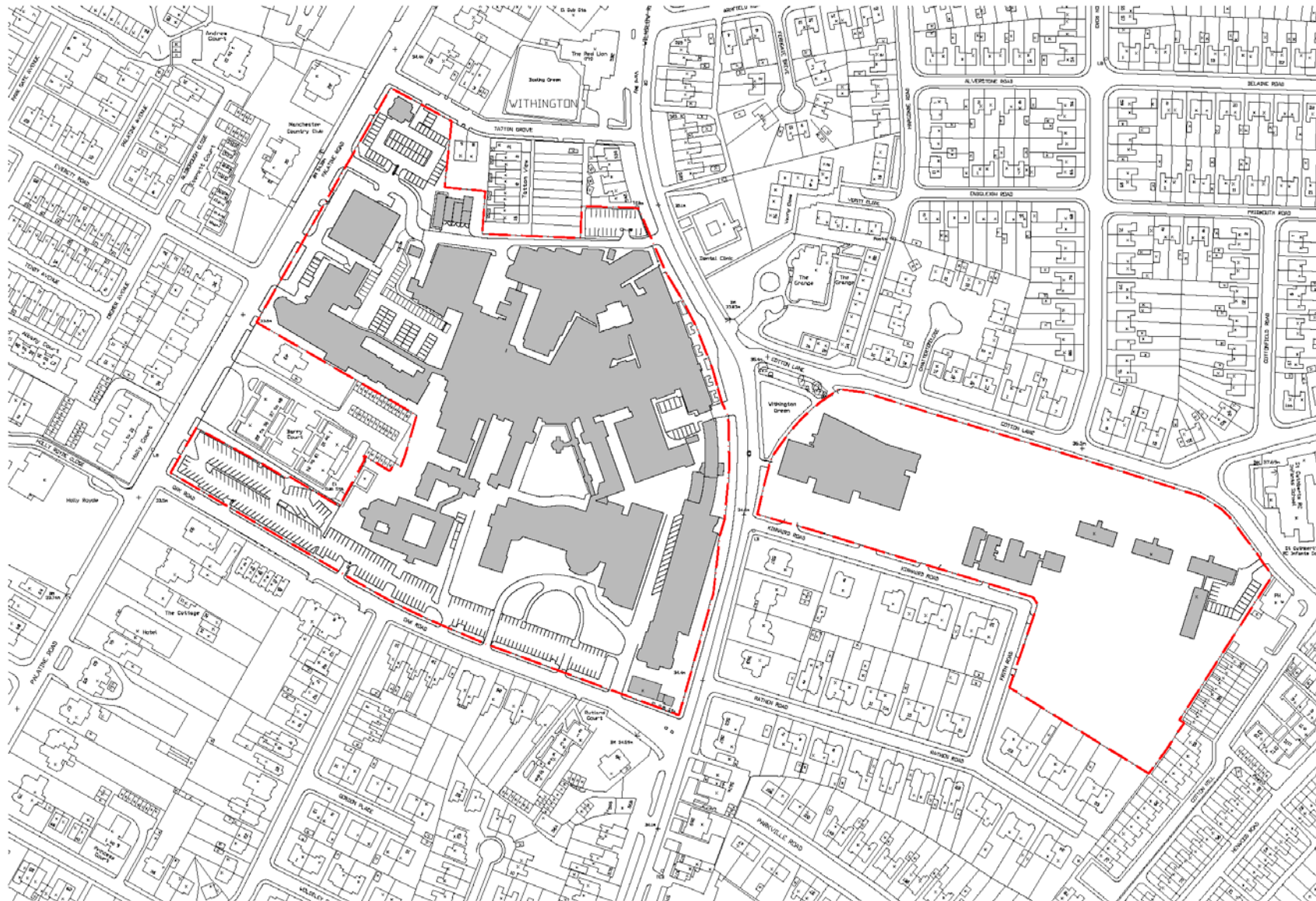
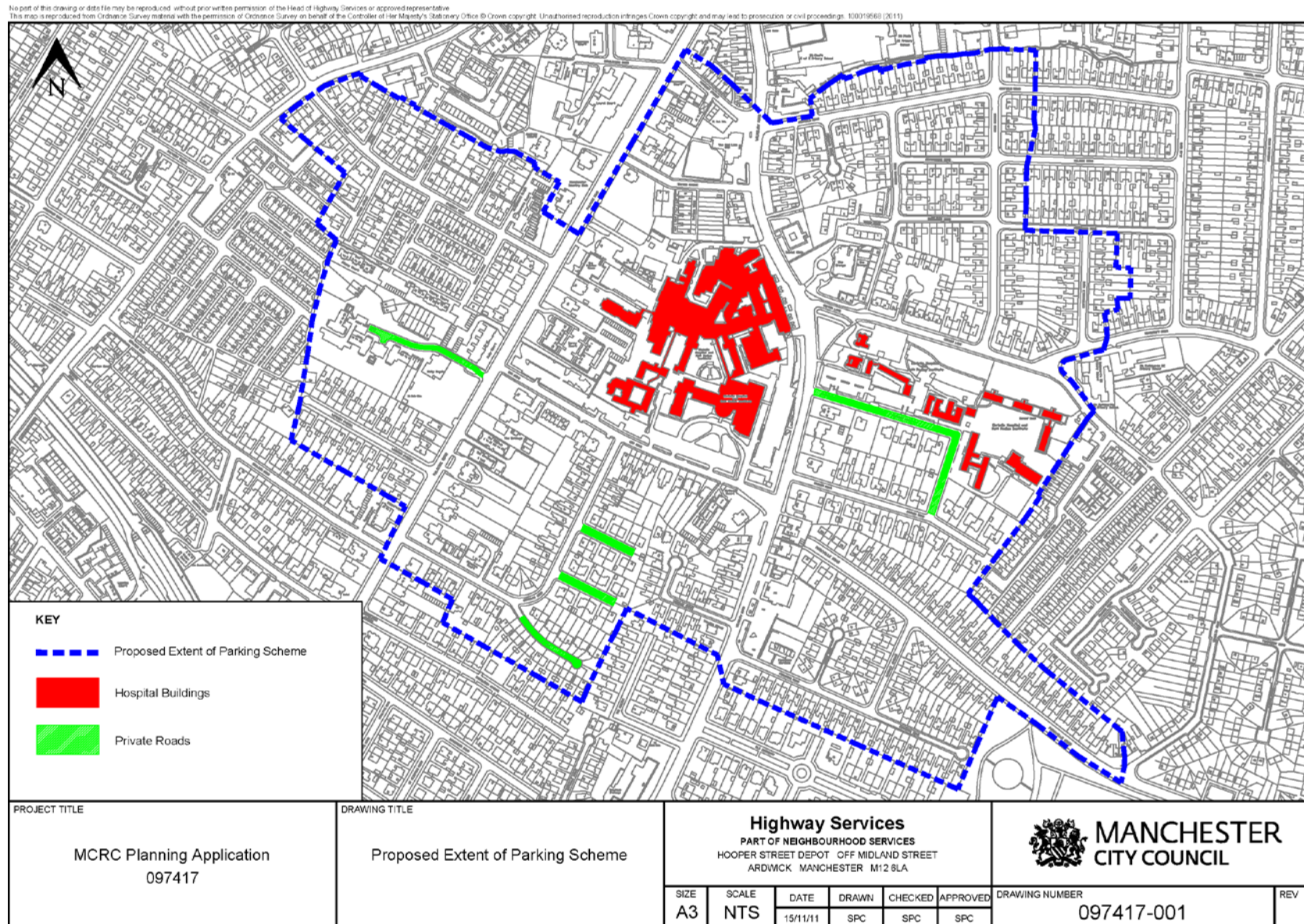


Figure 5.2. Restricted Parking Zone (for consultation)



6 THE OPPORTUNITY

Strategic Aspirations

- 6.1 As a leading cancer centre, the Trust continues to plan service provision that best responds to patient needs as well as technological and research advances. The Withington site will continue to play an important role in the Trust's plans supported by local provision through satellite facilities where appropriate. This includes the satellite radiotherapy centres already operational in Salford and Oldham, a pilot scheme that allowed patients in Bury to receive chemotherapy treatment at a health clinic, plus recently announced plans to offer chemotherapy through a mobile unit that will improve accessibility across the region. This mobile facility is forecast to treat 11,000 patients per year, allowing patients to access treatment close to home and thereby reducing patient numbers at the Withington site. These plans have been developed in response to public feedback.
- 6.2 All emerging development proposals focus on the need to support advances in cancer treatment and enhance patient survival rates. For example, NHS patients in the UK currently have to travel to America or Switzerland to access proton beam therapy (an approach that uses a particle beam to focus radiotherapy on cancer cells therefore minimising damage to surrounding material), but from 2018 it will be offered at The Christie. This is an important advance for patients, the Trust and the UK cancer healthcare offer.
- 6.3 The table below summarises the evolving service models for different strands of the Christie's service offer. This demonstrates the drive for continued innovation in research and healthcare delivery that in a number of cases will require the development of new or improved

premises (potentially both on and off site) to allow services to be offered efficiently and to a high standard.



Table 6.1: Evolving Service Models

Service	Evolving Model	Site Implications
Chemotherapy	Push for greater local provision and flexibility to improve service access. Examples include delivery through local doctors' surgeries and the launch of a new mobile unit. Currently, 53% of clinically suitable treatments are delivered locally with a target in place to raise this proportion to 80% by 2014/15, at a time of growing treatment numbers.	On-site delivery will continue to be focused on the Oak Road Treatment Centre with young people to benefit from the new integrated Young Oncology Unit.
Radiotherapy	Proposals are being prepared in response to a recognised need for additional radiotherapy capacity over the next five years. Continue delivery on-site and through a network of satellite centres, e.g. Salford and Oldham, to provide patients with more local and convenient provision. Ultimate intention to ensure no patient in Greater Manchester and Cheshire has to travel for more than 30 minutes to access treatment.	Proton Beam Treatment Centre proposed on the Christie site.
Specialist services	Continued focus on building/maintaining the critical mass of specialist expertise on site to build on existing specialisms in both research and clinical practice. For example by hosting a new proton beam therapy centre and building on strengths in Peritoneal Tumour Services.	Proton Beam Treatment Centre proposed on the Christie site
Education	The School of Oncology aims to become one of the country's leading providers of cancer education. Provision will include independent educational programmes and working in collaboration with other key education providers to ensure both staff on site and in specialist centres can deliver the highest quality of care. There is a desire to deliver more training on site as well as a larger e-learning programme.	Extension to the Education Centre on the site.
Research	Desire to continue building the research base on site to ensure alignment with clinical practice, thereby supporting prompt take up. External resources will continue to be attracted to support research programmes and infrastructure investments. Research functions will continue to grow through the attraction of new funding and the creation of new posts which may result in the need for new or improved premises over time.	New MCRC building on site. Potential reserved for a Phase 2 expansion.

Emerging Developments

- 6.4 In addition to the committed projects described in Section 5 there are also emerging and anticipated development projects that have been considered within the Framework. The exact location and physical requirements of these developments are not yet fully known and it will be necessary to review this Framework at regular intervals as detailed requirements become better known and potentially new projects may arise (as discussed in Section 1). In the meantime the Framework reflects reasonable assumptions about these emerging and anticipated development projects.
- 6.5 The Schedule of Development Projects (Table 6.2 overleaf) sets out the current knowledge on projected development projects at The Christie. The key physical projects that the current Development Framework seeks to accommodate are summarised below.
- 6.6 ‘**Committed**’ projects have already been allocated a site . They may be at various stages in the construction process. They include:
- YOU/HTU facility
 - MCRC Phase 1
 - Brachytherapy services - courtyard infill
- 6.7 ‘**Emerging**’ projects are those which have already been identified in terms of funding and programme, although not fully scoped and confirmed. The emerging projects identified are:

- **Proton Beam Treatment Centre** – a proposal for a specialised treatment centre for patients that currently have to travel abroad.
 - **Maggie’s Centre** – The Trust are in discussions with the Maggie’s Centre charity about providing one of their patient and relative support facilities at The Christie. This has been designed by Foster + Partners, and has secured planning permission.
 - **Ambulatory Centre** – 2-storey facility, to be developed alongside/within the Proton Beam Treatment Centre.
 - **Outpatients Department** - re-organisation
 - **Integrated Procedures Unit**
 - **Haematology OPD**
- 6.8 ‘**Anticipated**’ projects are considered to be aspirational projects that the Trust would like to deliver in the future. At present this includes:
- **MCRC Phase 2** – a site has been identified behind the new MCRC building for potential future expansion.

Table 6.2: Schedule of Development Projects *(as of June 2013)*

Project Name	Brief Description	Status	GIA m2	Investment £k	Comments
Critical Care Unit	4-storey critical care unit & Trust HQ offices	Complete		7,225	
Multi-Storey Car Park	4-storey, 216-spaces on Palatine Road	Complete		3,000	
Oldham Radiotherapy Satellite	Christie-branded ambulatory 3-linac radiotherapy unit	Complete		14,395	Development managed by Pennine Acute Hospitals Trust, financed by Christie
Oak Road Patient Treatment Centre	5-storey chemotherapy, clinical trials and private patients	Complete	6,650	35,480	
Salford Radiotherapy Satellite	Christie-branded radiotherapy centre with stereotactic radiosurgery capability	Complete	TBC	17,420	Development managed by Salford Royal, financed by Christie
Operating theatres	Conversion of existing internal space	Complete	895	4,130	
Education Centre	2-storey extension	Complete	TBC	1,200	
Additional administration accommodation	Candleford Road purchase	Complete		824	
Car park re-configuration	Interim works to maintain existing capacity	Complete	TBC	1,050	
Brachytherapy suite	2-storey courtyard infill	Complete	815	3,200	
MCRC Phase 1	3-storey research labs and clinical trials co-ordination unit	due to complete Oct 2014	5,980	29,700	Development managed by UoM, financed by UoM/Christie/CRUK
Integrated YOU/HTU	New 3-storey young oncology unit integrated with extended (additional floor) existing haematology & transplant unit	Complete	3,130	12,200	

Purchase of Golden Lion site	Use TBC	Complete		2000	
Proton Beam Treatment Centre	3-storey 3-gantry PBT facility 3-5 storey clinical building	Emerging	6,580	141,000	£125m funding from central government
Clinical Facility	clinical facility adjoining/within PBTC	Emerging	4,950	15,000	
Maggie's Centre	Drop-in holistic support services for cancer sufferers, relatives & friends	Emerging	600	3,000	Development led by Maggie's Caring Centres, designed by Foster + Partners
Out Patients Dept re-organisation		Emerging	TBC		
Integrated Procedures Unit		Emerging	TBC		
Haematology OPD		Emerging	TBC		
Surgical facility upgrade	Ward refurbishment and potential theatre expansion	Emerging	TBC	500	
Long-term parking solution		Emerging	TBC		
MCRC Phase 2		Anticipated	TBC		
3 rd Radiotherapy satellite		Anticipated	TBC		
Admin re-provision		Anticipated	TBC		
Overnight & residential accommodation		Anticipated	TBC		
New education centre		Anticipated	TBC		

Estimated Benefits

- 6.9 As well as supporting important advances in cancer treatment, The Christie's development proposals will make a significant contribution to economic growth. Planned developments will attract substantial new investment into the City. These are unique opportunities to capture mobile funding that would not be realised in the absence of new purpose built premises. As examples:
- The Proton Beam Treatment Centre will receive £125m from central government;
 - The Manchester Cancer Research Centre has attracted £10m of funding from Cancer Research UK; and
 - Maggie's will make a £3m investment in their new capital facility plus recurrent costs of £6m over 10 years.
- 6.10 These examples build on the Trust's successful track record of attracting external investment to support recent projects, such as the Oak Road Treatment Centre which attracted a grant of over £4m from Cancer Research UK.
- 6.11 Each of the planned developments outlined above has been attracted to the City on the strength of the Christie's existing reputation and plans for the future as well as Manchester's wider strengths in the health sector. Investments are mobile and there is a need to demonstrate to funders that the Christie can deliver, from a perspective of physical development as well as new and improved patient services and sector leading research. The development of new facilities also provides the opportunity for new research funding to be attracted into Manchester further raising the benefits. For example, the University of Manchester's successful £12.8m bid to the UK Research Partnership Investment Fund which will pay for
- 6.12 equipment as part of the Manchester Cancer Research Centre and forecast research income of £1.2m per annum by 2014/15 linked to the Proton Beam Therapy Centre.
- 6.13 Initial estimates show that developments will create over 550 new jobs in Manchester. For example:
- Manchester Cancer Research Centre – currently under construction and due to open in 2014 creating 60 jobs within the clinical trials unit and a further 70 posts through the University of Manchester; and
 - Proton Beam Treatment Centre – a £135m scheme due to be completed by 2018, supporting 120 jobs on-site.
- 6.14 This builds on the Trust's successful track record in employment creation, including the addition of 90 posts through the opening of the Oldham and Salford radiotherapy satellite centres.
- 6.15 Taking the full potential development portfolio (as outlined in Table 6.3), the following gross economic contributions are forecast⁴:

⁴All timescales based on previously discussed development blocks. Therefore 2012-14 developments are education centre, car park, brachytherapy suite, MCRC phase 1, integrated YOU/HTU and Maggie's centre, 2015-2018 developments are proton beam therapy centre and co located services and post 2018 developments are all remaining schemes. The forecast includes consideration of which developments will create new jobs (a large number of

Table 6.3: Forecast Economic Contributions

Timescales	Projects	New Jobs	Additional Contribution to GVA ⁵
2012-2014	Education centre, car park alteration, brachytherapy suite, MCRC phase 1, integrated YOU/HTU and Maggie's centre	140	£4.4m
2015-2018	Proton beam therapy centre and co located services	165	£5.2m
Post 2018	See Table 6.2	250	£7.8m

6.16 All of the jobs above are forecast to be additional to current levels. Some developments, e.g. the integrated young oncology unit, co located services and haematology and transport unit, will not generate new employment but will ensure that existing employment levels are sustained and the patient offer enhanced.

6.17 Construction programmes will also support the creation of temporary construction employment, helping to boost the sector at a time of challenging market conditions. The 2012-18 development programme is estimated to represent an investment of over £200m which will support 2,090 construction job years (209 construction job equivalents⁶).

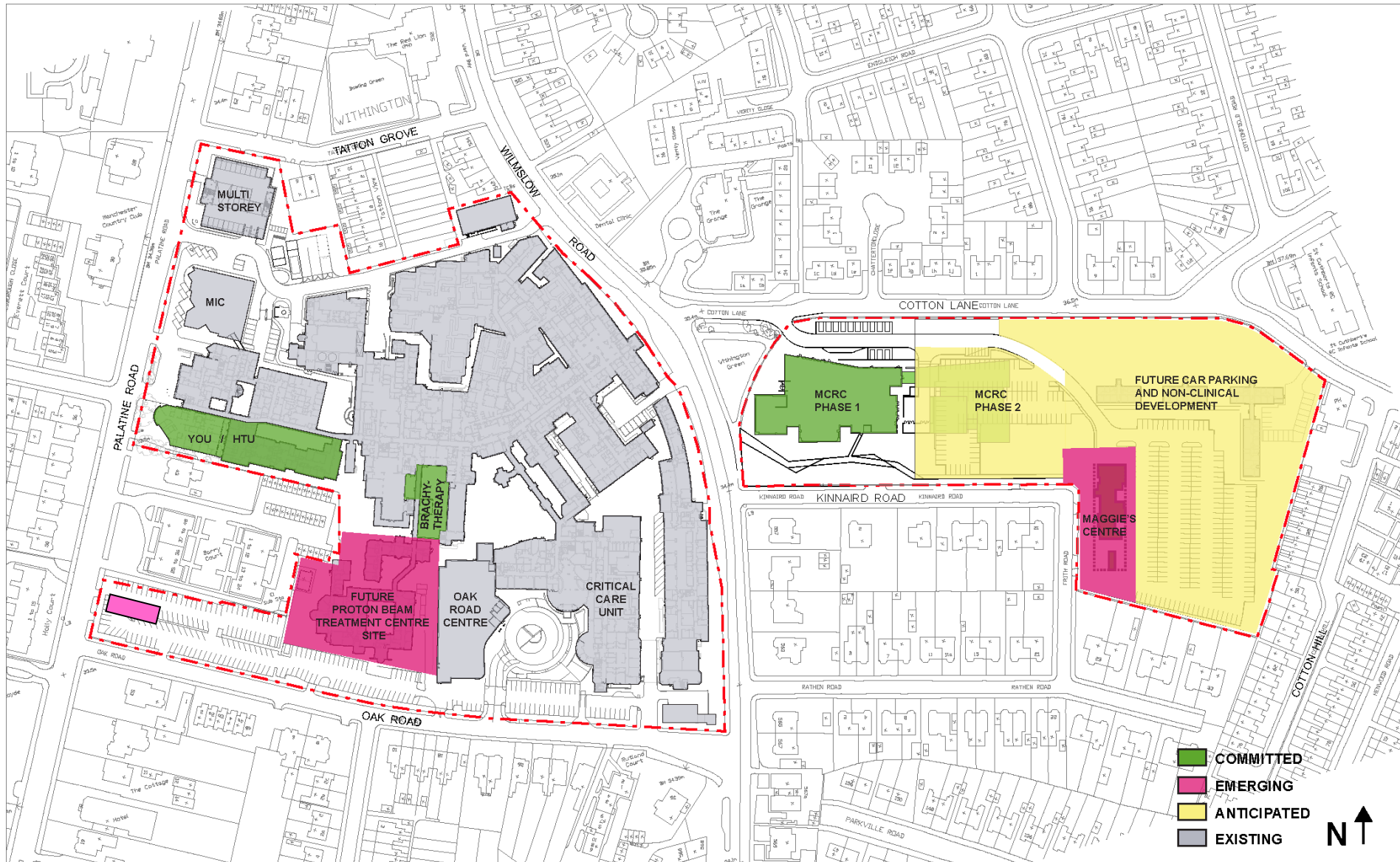
developments will accommodate relocated existing posts) and the broad numbers for each.

⁵ GVA benefits are calculated based on the average GVA contribution per FTE post in the North West Human Health and Social Work activities sector.

⁶ Assuming 10 construction job years equate to one construction job equivalent.

6.18 The Christie will be an important driver of employment creation within Manchester over the next few years. At a time when investment conditions remain challenging, the developments planned at The Christie will make an important economic contribution, both through the construction and operational phases. The jobs created by new and enhanced facilities will mirror the mix of employment already offered at the Trust, providing employment opportunities for a high number of local residents across a range of occupations and skills. The Local Employment Partnership (see 5.19-5.24) will play a significant role in ensuring that local residents benefit from the resulting employment opportunities. The planned developments will also attract new highly skilled individuals to Manchester and neighbourhoods local to The Christie.

Figure 6.1: Projected Development Projects (main site)



7 THE CHALLENGE

Issues to Address

- 7.1 This section describes the issues that must be addressed for the development aspirations of the Trust to be met. These challenges include issues of both development form and character; and transport and parking.

Local Character and Capacity for Development

- 7.2 The Christie site sits within an established residential area and on the boundary of Withington Conservation Area. When accommodating the development required it is important to ensure that issues of local character and residential amenity are not compromised. This means first understanding the character of the surrounding area and then positioning and designing new development in a manner that responds to this understanding.
- 7.3 Section 3 has set out our detailed understanding of the surroundings. This is also summarised in Figure 3.1. The Development Framework responds to this analysis according to 6 Design Principles.

Design Principles

1. Ensuring that the existing character of surrounding streets is preserved.
2. Maintaining and enhancing existing street frontages by respecting established boundary treatments, landscape treatments and building set-backs and following the established relationship of buildings to the street.

3. Establishing parameters for development that identify the maximum height and scale of new built form that can be accommodated without prejudicing local character or amenity.
4. Ensuring that important street views that contribute to local character are not adversely affected by development.
5. Ensuring that the setting of the Conservation Area, and the of the Grade II listed Red Lion PH, are not adversely impacted.
6. Where existing properties back onto the site ensuring that residential amenity, privacy and security to these dwellings is maintained by the boundary treatment, landscape buffer and position and scale of new buildings.

- 7.4 The Development Framework (see Section 8) responds to all of these points.
- 7.5 First it is necessary to consider where on the site new development may be located. This is a highly developed site with little obvious space for new development. In order to meet the requirements and aspirations of the Trust a considered and creative approach is necessary.
- 7.6 Sites for new Christie development are likely to be on currently undeveloped parts of the site or on parts of the site that could be redeveloped at a greater scale without adversely affecting the issues described above. These are the potential principal development zones. There are also likely to be smaller scale infill and redevelopment sites within the existing built-up area that cannot yet be identified.
- 7.7 Following the recent acquisition of the Golden Lion and Candleford Road sites , there is no current strategy for further land acquisitions.

Also, it should be noted that this Framework concerns the main Christie site in Withington and does not cover development by the Trust at other sites (e.g. the satellite centres at Oldham and Salford), which remain an important part of the Trust's wider strategic operations.

- 7.8 Some of the potential development zones are currently used for car parking. Where spaces are lost this loss will need to be considered and mitigated for as part of the Transport and Parking Framework (see Section 9).
- 7.9 The Framework focuses principally on the edges of the site, ensuring that development will not prejudice surrounding street character or have adverse impacts on neighbouring properties. However, there may also be opportunities for redevelopment within the core of the main site (between Palatine Road and Wilmslow Road) in locations that do not directly neighbour surrounding properties and are consequently less sensitive. Development here could be at a greater scale and height, although it would still have to be appropriate in terms of local character. Such redevelopment will be assessed on a scheme-by-scheme basis as opportunities emerge.
- 7.10 The principal development zones comprise:

- 1. The larger part of the Kinnaird Road site, to the east of the MCRC. This area is mostly occupied by car parking at present but also includes a number of uses in outdated buildings that could potentially be accommodated in better accommodation. A large part of this site could be developed at an appropriate scale in keeping with its surroundings, whilst respecting set back distances, providing suitable landscape buffers and retained mature trees. Development of this site would be fully integrated with thinking on

the Transport and Parking Framework as it is likely to displace many spaces.

- 2. Under-utilised parts of the Christie site facing Oak Road (including the soon to be replaced YOU).
- 3. Palatine House (see photograph below) – this is in active Christie use but is only a two-storey building and is set back further from the street than the established building line. A larger scale building could potentially be accommodated here if required.



Palatine House

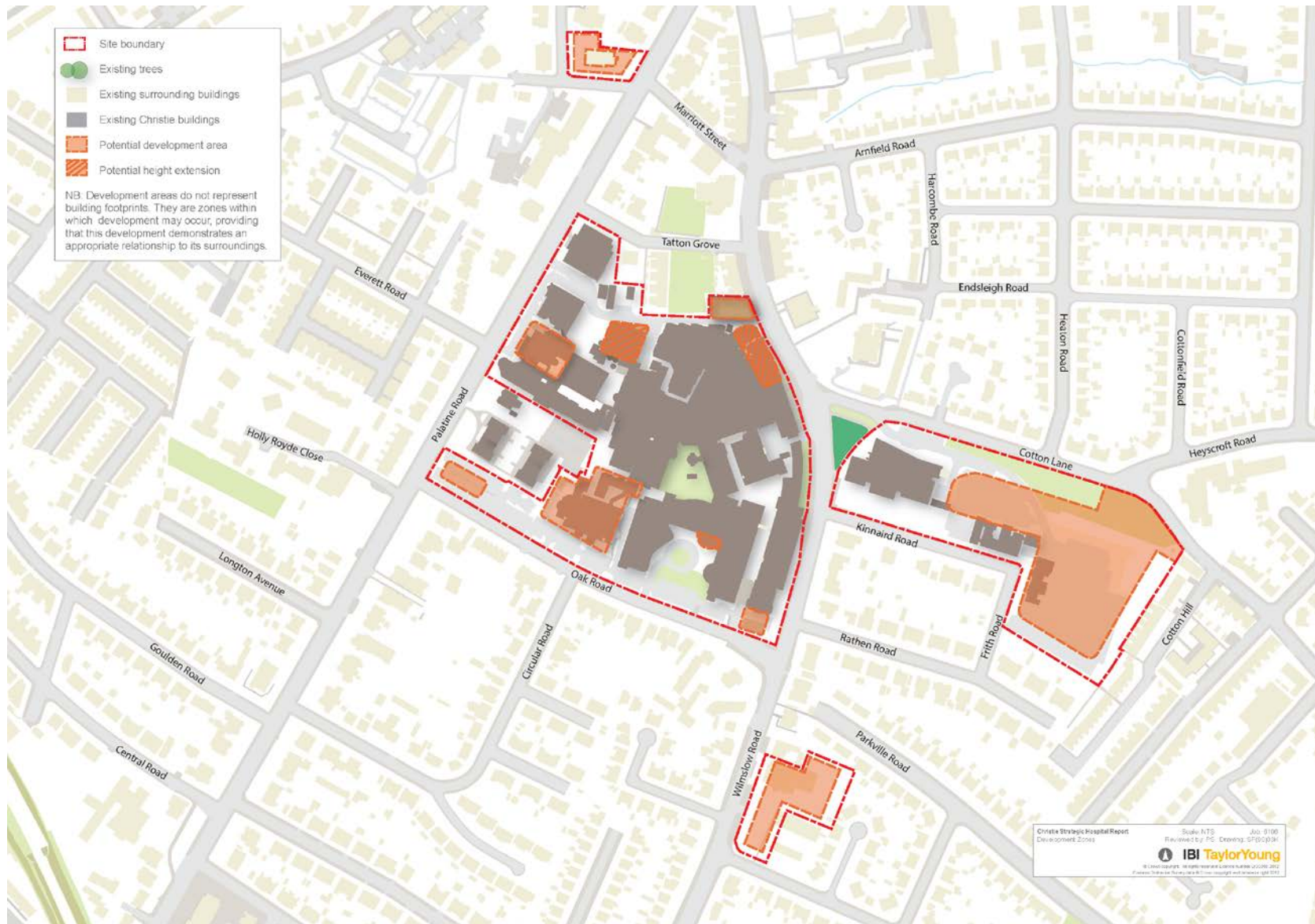
- 4. The site fronting Wilmslow Road that is currently occupied by temporary buildings. In addition to considering an additional storey on the two storey element of the original frontage building.



Temporary buildings on Wilmslow Road

5. The Mortuary and Estates building, which is relatively low-rise and may have potential for an extension in height.
 6. The property recently acquired by the Trust on Candleford Road. This comprises a Victorian building converted to office use, and its grounds.
 7. The site formerly occupied by the Golden Lion PH, now a cleared site, on Wilmslow Road, also recently acquired by the Trust.
- 7.11 The guiding development principles, in Section 8, provide further guidance on how these sites should be developed in order to meet the criteria discussed earlier (at paragraph 6.3).

Figure 7.1: Potential Development Areas



Transport & Parking

Travel by Patients

- 7.12 Due to the nature of the illnesses which the Christie deals with, it treats patients with the utmost sensitivity. This includes any attempt to influence the way patients travel to the site and while much information is available to patients about how they can travel to the site the Trust's policy is to accommodate patient parking on site and as close to the point of treatment as possible.
- 7.13 The current arrangement for patient parking provides spaces on both the Palatine Road and Kinnaird Road sides.
- 7.14 There are currently about 310 patient parking spaces on site which are controlled by a pay and display system. 32 of these are for disabled patients. There are some 330 patients parking on the site at the current time.
- 7.15 Patient parking also takes place on the streets surrounding the site. The surveys of on street parking indicate that there is a maximum of around **130** patients and visitors parked at any one time. On this basis the approximate demand for patient/visitor parking is **460** spaces.
- 7.16 It is the Trust's intention to meet this level of patient demand on site in car parks on the Palatine Road side.
- 7.17 The Trust do not expect there to be a significant increase in the numbers of patients visiting the site. Of the new proposed developments it is only the Proton Building that will result in additional patients being treated at the site. It is anticipated that this will generate the requirement for 16 additional patient parking spaces.

Travel by Staff

- 7.18 The Christie has been monitoring how staff travel to work since 2005. Table 7.1 below shows how the proportion of staff using each mode of transport has changed in this period. Acknowledging that there are some statistical effects in the figures the overall trend is for a reduction in the number of staff who drive to work (nb. The 2010 survey had a low response rate and has not been used in this analysis as it is not considered representative). This trend has been encouraged by the site's Travel Plan which has been in operation since 2005 and as referenced above has been given a 'Green Apple' award.

Table 7.1: Staff Travel Patterns

	March 2005	May 2006	July 2009	2011
Public Transport	9%	9%	13%	12%
Cycling	5%	7%	7%	6%
Walking	9%	12%	14%	15%
Car Share	6%	6%	7%	5%
Car Driver	71%	66%	59%	63%

7.19 The most recent mapping of staff postcodes was undertaken in 2011. In terms of the distances travelled to work Table 7.2 below sets out a summary of the data.

Table 7.2: Distance Travelled to Work Based on Postcode Analysis (AECOM 2011)

Distance to Work	Number of Staff	Percentage
Under 2 kilometres	416	18%
2-5 kilometres	505	22%
5-10 kilometres	623	27%
Over 10 kilometres	734	32%
	2278	100%

7.20 Based on an analysis of staff surveys Table 6.3 sets out the distance travelled to work by car drivers. [NB. the sample size is low].

Table 7.3: Distance travelled by staff driving to work (AECOM)

Distance Travelled by Car Driver	Number of Staff	Percentage of all Drivers
0-1 miles	4	3%
1-2 miles	13	8%
2-3 miles	30	19%
3-5 miles	28	18%
5-10 miles	44	28%
10-20 miles	22	14%
20-30 miles	14	9%
30+ miles	5	3%

7.21 Based on a further analysis of postcode data undertaken in 2011 using the Accession software (this allows journey times by public transport to be analysed) the Table 7.4 below sets out the journey times by public transport for staff at the Christie. This includes all staff not just those who drive.

Table 7.4: Public Transport Journey Times from Accession (AECOM)

Public Transport Journey Time	Number of Staff	Percentage
0-10 minutes	132	6%
11-20 minutes	378	17%
21-30 minutes	366	16%
31-40 minutes	362	16%
41-50 minutes	227	10%
51-60 minutes	246	11%
Over 1 hour	567	25%

The Green Travel Plan

- 7.22 The Christie has had a travel plan in place since 2005. The travel plan has helped reduce the number of staff members who drive to work and has won a Green Apple Award for environmental best practice.
- 7.23 Despite this and due to the fact that there is a plentiful supply of free (no cost) on street parking surrounding the site, the travel plan to date has been trying to achieve two conflicting aims. On the one hand it is aiming to increase the number of people who use sustainable travel modes to get to work while on the other hand it is trying to maximise the use of on-site parking to limit the number of people who park on street. A travel plan requires 'sticks' which discourage people to drive and 'carrots' to encourage people to use other modes of transport. With the current parking arrangements it is very difficult to provide a sufficiently strong parking "stick" to give any "carrots" a reasonable chance to be effective.
- 7.24 The implementation of the Section 106 parking scheme will change this and will make the implementation of a stronger carrot and stick approach more effective.
- 7.25 In this context the Christie is committed to implementing a revised and stronger travel plan and the process of preparing and implementing the plan is ongoing. The latest version is appended to this document
- 7.26 The data presented above suggests there is scope to influence a potentially significant change in travel behaviour with the right balance of carrots and sticks in the revised plan. The key headline figures are :
- **18% of staff live within 2km of the site. 2km is recognised as a relatively achievable walking distance for most able and moderately fit people. Given that 15% of staff are already walking to work there is probably limited scope to increase the proportion of staff walking.**
 - **40% of staff live within 5km of the site. 5km is recognised as a relatively achievable cycling distance for most able and moderately fit people. As only 6-7% currently cycle there is potential scope to increase this proportion.**
 - **55% of staff live within a 40 minute public transport journey of the site. 40 minutes is generally recognised as an acceptable journey to work time. As only 11-13% of staff are currently using public transport there is potential scope to increase this proportion.**
- 7.27 Set out in the sections below is a brief summary of the provision for each major mode of transport. This is followed by a consideration of the potential mode splits that could be achieved. Section 9 discusses recommendations as to how the Christie Transport Strategy can respond to these points and provide sustainable modal shifts (i.e. by providing 'carrots').

Public Transport

- 7.28 The Christie is located on the Wilmslow Road bus corridor which is renowned for its high frequency bus service. The services split between Palatine Road and Wilmslow Road and there are bus stops conveniently located on both roads. The Palatine Road buses serve districts such as Northenden, Sale and Wythenshaw. The Wilmslow Road buses serve Didsbury, the Heaton and Stockport.
- 7.29 To the north of the Christie the buses run pretty much exclusively on the Wilmslow Road corridor so while the population in this corridor has very high bus accessibility to the Christie, away from the corridor there is a reliance on connecting services which makes bus travel less attractive particularly under current ticketing arrangements.
- 7.30 The same point about relying on connecting bus services applies to rail as well, with perhaps the most convenient connections available in the City Centre although the connection between heavy rail and bus in the City Centre is not currently particularly convenient.
- 7.31 Significant investment in public transport provision is on-going across Greater Manchester and this will benefit the Christie. The Metrolink connection to Didsbury opened in 2013 and the West Didsbury station on Lapwing Lane now serves the Christie site.
- 7.32 The cross city bus services and the introduction of consistent ticketing between all operators including an Oyster type ticket will also be significant. It will extend direct bus services and make connections, including between modes more convenient.
- 7.33 The Christie is committed to working with TfGM, MCC, and public transport providers through its Travel Plan to make the most of these significant changes in public transport.

Cycling

- 7.34 South Manchester has an established culture of cycling. The terrain is flat and a lot of the road network has specific cycling facilities. There are already a relatively significant number of people who cycle to work at the Christie.
- 7.35 On site there are now 146 cycle parking spaces spread over a number of locations. 94 of these are covered stands. There are staff showers on site but only in one location.
- 7.36 The Christie recognises that provision for cyclists can be improved through the provision of more covered cycle stands and additional showering and changing facilities located at a number of locations across the site. More cycle parking stands will be required if the proportion of cyclists increases significantly.
- 7.37 Manchester City Council have targeted Wilmslow Road and Palatine Road as priorities for an enhanced maintenance regime for cyclists as part of the Greater Manchester Cycling Ambition City Grant submission (co-ordinated by Transport for Greater Manchester). The aim is to deliver continuous, dedicated, segregated cycle lanes wherever possible. If sufficient funding is unlocked this would offer significant improvements for cycle travel to The Christie.

Walking

- 7.38 Walking is the second most commonly used mode of transport for staff and has seen the most significant increase since 2005. The local area is conducive to walking. In a physical context there is not

much the Christie can do to encourage more staff to walk although the provision of improved showering and changing facilities could encourage walking / running from further afield. Of course, there is also an element of walking combined with public transport use.

Motorcycles

- 7.39 There are five motorcycle parking hooks available on the site. There is very little travel to the site by motorcycle and the original travel surveys showed little interest in motor cycling. In this context the Trust is not pursuing any measures to encourage further use of motorcycles. Should a demand for motorcycling materialise in the future the Christie would review this approach.

Car Passengers (Car Sharing)

- 7.40 Some staff car share and there are now 29 car sharing spaces on site and a specific car share permit has to be obtained to use this car park. For a site employing so many people there should be the opportunity for more to car share. In this context it may be appropriate to dedicate more spaces for sharing.
- 7.41 Car share spaces for staff are now located in more convenient locations giving an additional incentive to car share

Car Drivers

- 7.42 There are currently 480 staff parking spaces on site of which 12 are disabled spaces. Staff require a permit to park on site for which there is a small charge made each month. Staff have to apply for a permit. There has been a freeze on the issue of new permits for the past year or so while the travel plan is reviewed and developed. A

considerable amount of staff parking also occurs on the streets surrounding the site.

- 7.43 As reported in Section 5 at the time of writing there were about 470 staff parking on site and a further approx 900) on street.
- 7.44 With the implementation of the strategy to relocate all patient/visitor parking to the Palatine Road site there will be a reduction in the onsite provision for staff. Once the parking rationalisation works are complete on the Kinnaird Road side there will be some 430 spaces available for staff on site.
- 7.45 This clearly represents a reduction in on-site parking provision for staff. Considering this alongside the implementation of the Section 106 on street parking scheme the journey to work by car will become significantly less convenient. As said previously, The Christie recognises this as an opportunity to re-launch a revised and stronger green travel plan and the Plan will include measures (sticks) to reinforce the constraints for staff parking.
- 7.46 The Christie's aim is to attempt to be self-sufficient in terms of parking supply and thereby minimise parking on street to avoid the conflicts with local residents which have been a feature of the site for many years. A main element of achieving this aim is the implementation of the revised and stronger Green Travel Plan. Alongside this however, additional off street car parking may be required.

Potential Mode Split

- 7.47 In Section 9 a summary is provided of the measures The Christie is intending to introduce into the revised Green Travel Plan and these have been touched upon in the commentary of each travel mode above. This, considered alongside the very significant change to the

parking environment for staff, will mean the Christie has a strong Green Travel Plan in place which, if implemented effectively, should result in further significant modal shift and a reduction in the number of single occupancy staff cars.

- 7.48 Research was undertaken a number of years ago into the effectiveness of Travel Plans at hospitals. This indicated that the most effective travel plan was implanted at Derriford Hospital in Plymouth where a 24% (78%-54%) reduction in car use was achieved over a 6 year period.
- 7.49 At Nottingham City Hospital a 17% (72%-55%) reduction was achieved over a 3 year period. A 4% (58-54%) reduction was achieved at The Radcliffe hospital in Oxford over a one year period. The Department for Transport's Report making "Travel Plans Work" (2005) indicates that the most effective Travel Plans could potentially result in a 30% reduction in single occupancy car use over a three to four year period.
- 7.50 The Christie has already achieved an 8% reduction in the number of staff who drive to work in the six years between 2005 and 2011 and this was in an environment with little or no restraint to convenient car parking.
- 7.51 From the 2001 census (2011 info not available at this time) for the Manchester Local Authority District the proportions of residents using the main modes of transport to get to work were:-
- Walking 12%
 - Cycling 3.5%
 - Car share 7%
 - Public Transport 25%
 - Car Driver 49%

- 7.52 The future modal split of staff travel to the site is very difficult to assess. Based on the post code analysis the mode with the greatest scope to achieve a significant increase in use is public transport where the site will also benefit from the public investment that is being made in Metrolink and other public transport schemes. A challenging but potentially realistic target for public transport use over a 3-5 year period would be a 20% mode share. This represents 9% more staff using public transport or an 82% rise over the existing levels.
- 7.53 The level of car sharing at the site is low and could also increase with the right travel plan measures (focused on priority parking). Over the years the staff travel surveys have been undertaken the proportion of staff car sharing has varied between 5 and 7%. It should be possible to increase this to about 10% over a relatively short timeframe.
- 7.54 Similarly with cycling the proportion of staff has varied between 5 and 7%. This is already a relatively high proportion but is achieved on a site with relatively limited facilities for cyclists. In this context it should be possible to increase the proportion of people cycling to about 10% although this is a more challenging target than the increase in car sharing.
- 7.55 There is probably little scope to increase the proportion of walkers as at 15% this already represents a similar proportion of the staff who live within 2km of the site.
- 7.56 In this context the future proportion staff driving to work would be between 45% and 50% based on challenging targets for increases in the other modes. At none of the other hospital sites referenced above was the proportion of sole drivers less than 50%. We believe this mode split represents a challenging but realistic target for use by staff at least in a short to medium timeframe (3 to 5 years).

Future Parking Demand

- 7.57 Using a 63% mode split this generates a staff parking demand of some 1,300 spaces. This indicates that there were about 2,050 staff on site at the busiest time within the survey period. As a cross check this represents about 75% of the total number of staff which is considered reasonable. For the purposes of this assessment we are assuming that 80% of staff will be on site at any one time.
- 7.58 In this context with a 50% car driver mode split the demand for staff parking would be about 1,025 spaces. If the modal split was 45% then the demand for staff car parking would be about 920 spaces.
- 7.59 Table 6.3 (in Section 6) presented an estimates of new jobs associated with future developments at The Christie. 140 new jobs are anticipated to be created over the next 2 years. With the 50% mode split this would create a demand for around 55 additional spaces (or 50 spaces at 45% mode split). Between 2015 and 2018 it is anticipated that a further 165 new jobs will be created. With the 50% mode split this would create a demand for around 65 additional spaces (60 spaces at 45% mode split).
- 7.60 Beyond 2018 it is anticipated that a further 250 new jobs will be created. With the 50% mode split this would create a demand for around 100 additional spaces (90 spaces at 45% mode split).

Table 7.5: Existing Parking Supply

	Location	Parking Spaces
Staff	Kinnaird Road	408
	Block C car park	21
	Palatine Centre	28
	Total	457
Patients/Visitors	Oak Road	166
	Palatine Road Multi-Storey	209
	Palatine House	5
	Total	380

Table 7.6: Projected Staff Parking Demand (based on 50% modal shift)

	New jobs	Staff on site	Modal Split (% cars)	Parking Demand	Shortfall in spaces (based on 457 current staff spaces)
Current		2,050	63%	1,300	840 (after s106 is implemented)
Revised Travel Plan target met, no growth		2,050	50%	1,025	570
Phased Growth with Travel Plan Improvements					
End of 2014	140	2,162	60%	1,295	840
End of 2018	165	2,294	55%	1,260	800
Beyond 2018	250	2,494	50%	1,245	790

NB. Projected parking demand and shortfall is given to nearest 5 spaces

Table 7.7: Projected Staff Parking Demand (based on 45% modal shift)

	New jobs	Staff on site	Modal Split (% cars)	Parking Demand	Shortfall in spaces (based on 457 current staff spaces)
Current		2,050	63%	1,300	840 (after s106 is implemented)
Revised Travel Plan Target met, no growth		2,050	45%	920	465
Phased Growth with Travel Plan Improvements					
End of 2014	140	2,162	58%	1,255	800
End of 2018	165	2,294	50%	1,145	690
Beyond 2018	250	2,494	45%	1,120	665

-
- 7.61 On this basis with a challenging Green Travel Plan target for car drivers of between 45% and 50% the future total staff parking demand would be between 1,120 and 1,245 spaces. At present there is car parking on the site for 837 spaces, 457 of which are for staff. There is therefore a projected shortfall of around 665-790 staff spaces, assuming a 45-50% modal split is achieved.
- 7.62 There are currently 380 patient / visitor spaces on site. This has increased recently from a longstanding provision of some 310 spaces. The demand for patient parking is about 460 spaces, with the balance of parking taking place on street. There is unlikely to be a significant increase in the number of patients/visitors to the site going forward with only the Proton Building likely to generate the demand for additional patient / visitor parking and this would only be in the order of 16 spaces. The Maggie's Centre will primarily serve patients already visiting the site as part of a linked trip. The Christie anticipate that the balance of patient / visitor parking will continue to take place on street. Discussions with the City Council are ongoing about how the residents parking scheme can be designed to facilitate this.

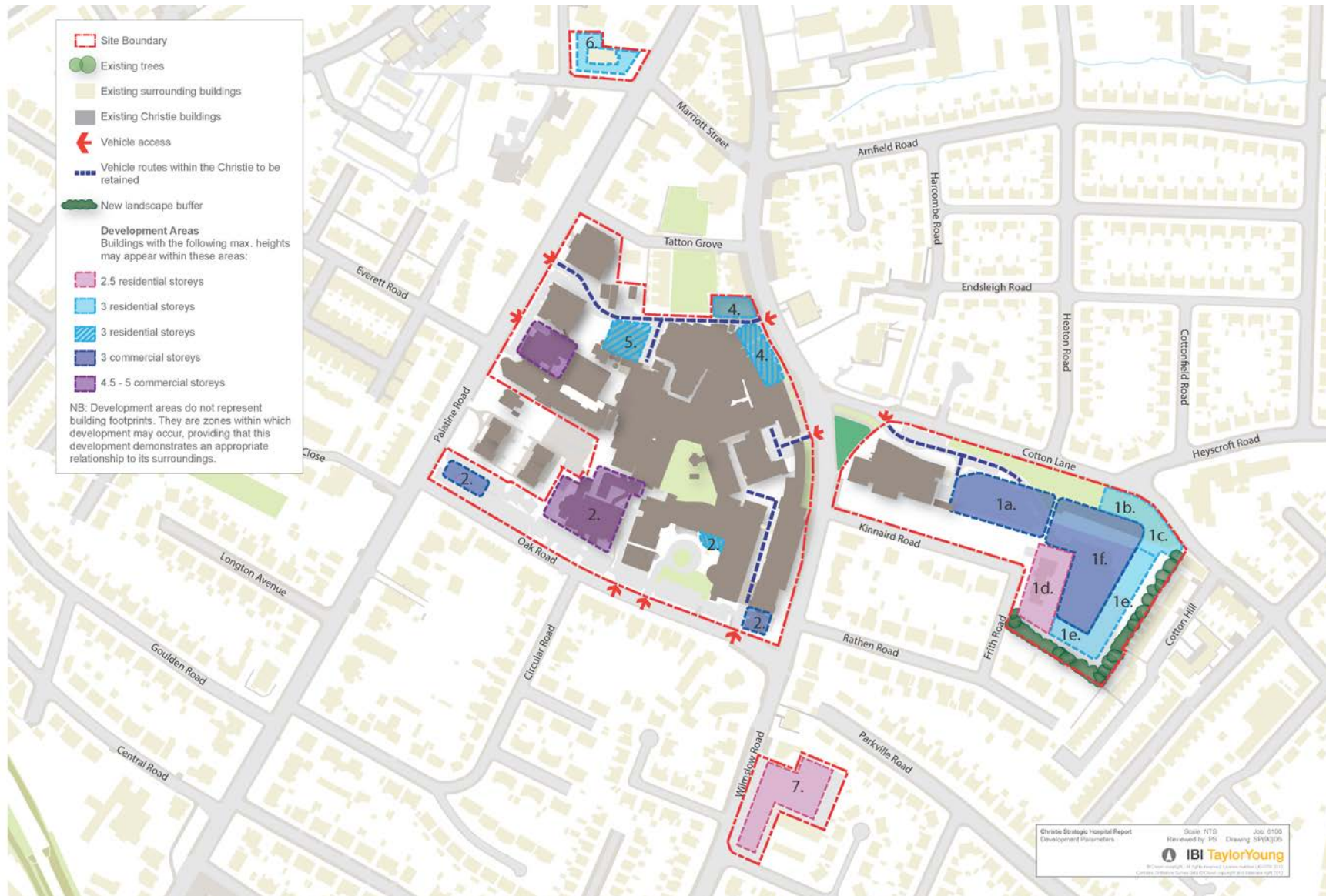


8 DEVELOPMENT FRAMEWORK

Development Framework

- 8.1 The analysis explained in the previous sections of this report has led to the preparation of a Development Framework for The Christie site. This has identified parameters for development that will allow the Trust to continue meet its objectives whilst ensuring that issues of local character, residential amenity and traffic and parking are not adversely affected.
- 8.2 The Development Framework is illustrated overleaf as Figure 8.1. This follows the approach described in Section 7 by identifying the parts of the site with development potential. For each of these the Development Framework indicates maximum area boundaries, and building height parameters for potential development. It is important to note that these are maximum parameters and do not represent continuous building footprints and site coverage within these zones. It should also be noted that any development would also have to respond to the Transport and Parking Framework before it could be considered appropriate. This is especially the case where new development will displace existing parking and generate an additional parking demand.
- 8.3 The Framework identifies seven principal development zones. It also recognises that there will be a need for ongoing refurbishments and smaller scale infill, and potentially redevelopments of parts of existing buildings. As described in Section 7, there is also potential for a greater scale of redevelopment in the core of the site, away from neighbouring properties. The scale and form of development here would have to be assessed as such opportunities emerge.
- 8.4 The principal development zones are:
1. Kinnaird Road Site
 2. Oak Road Sites
 3. Palatine House
 4. Wilmslow Road
 5. Mortuary and Estates Building
 6. Candleford Road
 7. The Golden Lion
- These are zones within which development could be accommodated, **they do not represent building footprints.**
- 8.5 During the life of this Framework other development zones may emerge and it may be necessary to review and update this Framework. In terms of the current principal development zones the Development Framework sets the following parameters.
- 8.6 It should also be noted that the Development Framework is concerned with the scale and massing of development. It does not consider elevational appearance. This is an important consideration, but one which is best left to the architects of individual development schemes, and assessed as proposals emerge. It will be important to respond to the local character of the area and preserve the distinctive parts of the Christie's character (e.g. the 1930s frontage on Wilmslow Road) whilst also delivering high quality design that meets specific clinical and operational requirements.

Figure 8.1: Development Framework



1. Kinnaird Road Site

- 8.7 This is the largest and most complex zone, and also the most sensitive. The site has the potential to accommodate a range of uses such as Phase 2 of the MCRC, the Maggie's Centre, improved accommodation for existing administrative functions, and staff parking in response to projected requirements from the Transport and Parking Framework , and potentially other uses. Apart from the recently approved Maggie's Centre the extent of these requirements and their positioning within this site is not confirmed at this stage. The development parameters set a framework within which these requirements could be met without compromising residential amenity and local character.
- 8.8 These parameters vary across this site, in accordance with the changing site conditions and interface with the surrounding environment, as assessed in Section 2. In general the Kinnaird Road site has the potential to accommodate development of a massing that reduces in scale eastwards, from the large MCRC building to a domestic scale development towards the sensitive eastern edges of the site. The labels below relate to those indicated on Figure 8.1. All parts of the site will continue to be accessed by the existing access road that runs parallel to Cotton Lane.
- 8.9 **(1a)** The site immediately behind the MCRC has been previously identified for a Phase 2 expansion of this facility. The large set back from Cotton Lane, with its strong boundary treatment, and the large set back from Kinnaird Road and boulevard of trees at this edge, means that a building of similar scale to the MCRC, following the same building line on both sides, would be appropriate. This would indicate a development of around approximately three commercial-sized storeys..
- 8.10 **(1b)** To the east of this, the Cotton Lane frontage offers potential for development of around three residential-sized storeys (approx 9.5m to ridge height). This would not adversely affect the character of Cotton Lane due to the wall, mature trees, and the distance to properties across Cotton Lane. The existing 3 storey blocks could potentially be demolished and replaced by new development here. The building line could come forward from the position of these blocks but should seek to retain the mature trees where they are in good condition. A detailed tree survey should provide guidance on the appropriate building line. The wall should be retained along this frontage.
- 8.11 **(1c)** An attempt should be made to provide greater presence, and an active frontage on the north-eastern corner of the site, in order to provide a presence on the street and a pedestrian entrance. There is a break in the tree belt that would allow this, though a small portion of the wall would need to be removed. The presence of the school opposite this part of the site establishes a context for buildings of a larger footprint than domestic housing. A height equivalent to three residential storeys would be appropriate here.
- 8.12 **(1d)** The edge of the site facing Frith Road is more sensitive, with a strong relationship to existing houses on Kinnaird Road and Rathen Road. A scale of development of two-and-a-half to three domestic storeys to match the height of these tall houses would be appropriate. Development within this zone would not be continuous. Buildings should be set within a landscaped area. The boundary treatment should match that of the existing streets: i.e. a low brick wall with openings and planting to soften the street scene. Development should front Frith Road and include a pedestrian entrance here. This site offers strong potential for a well designed and well-sited building to terminate the vista that is created by the trees on either side of

Kinnaird Road (see photograph below). Maggie's Centre has now been approved on this site.



Vista down Kinnaird Road

- 8.13 **(1e)** The frontages to the rear of properties on Rathen Road and Cotton Hill are more sensitive. Residential amenity, privacy and security must be respected by any development along this frontage, which should not be fully continuous. This will necessitate locating development at an appropriate set back from these properties with a strong landscape buffer 9-14 metres wide being provided along this interface. Any development here should be of a similar scale to these properties, i.e. the equivalent of two-and-a-half residential storeys.
- 8.14 **(1f)** This leaves the interior of this site. At present this is used for car parking. If the surrounding areas are developed, and landscaped as discussed above, then there is potential for this area to be developed without adversely affecting local character or residential amenity. Given the increased distance from the edges and the removal of any direct relationship to a street, this site could accommodate development of up to three commercial-sized storeys. Given the impact on existing parking spaces here, and those lost on the parts of

the site described above, this area is likely, at least in part, to be required to provide a car parking solution for wider site, in accordance with agreed Transport & Parking Strategy. This could involve a limited amount of multi-storey car parking to be provided within the parameters set out above.

2. Oak Road Sites

- 8.15 The Oak Road site has excellent functional relationships with existing clinical accommodation and would represent a very effective site for the Proton Beam Treatment Centre development. If this site was redeveloped then the new development should follow the building line established by the recently built Oak Road Centre adjacent to it. Development here could reflect the height of the Oak Road Centre (approximately four and a half commercial-sized storeys approx 20m in height). The set back from Oak Road and the existing established planting on the street boundary will ensure that it will not adversely affect the character of this street, as indeed has been proved by the neutral impact of the Oak Road Centre.
- 8.16 The Framework also considers the potential for smaller scale development at either end of Oak Road. These would serve to 'bookend' the Christie development on this side of the road and articulate these corners. These developments would be set further forward, matching the set-back distance of the residential properties on the other side of the road. They would be lower in height than the main Christie development on Oak Road, coming down in height to address the houses opposite.
- 8.17 On the eastern corner site there is currently a substation. This may become replaced by a new substation as part of Proton Beam Treatment Centre development, leaving this site available for redevelopment.

- 8.18 The western corner site is currently part of the visitors' car park. The availability of this site would be dependent on the wider Transport and Parking Framework. If these spaces were no longer required then development should be set back from both Oak Road and Palatine Road, retaining planting at the perimeter. The set-back to Palatine Road will need to be agreed; at present it is varied in this block. It is suggested that development here follows the building line of the existing Christie buildings (and as proposed for development zone 3), Oak Road would represent a logical point for the building line to step back, as it does to properties on its southern side.
- 8.19 Additionally, the main entrance to the Christie on Oak Road is currently a single storey build-out from the main elevation, within the entrance courtyard. This could be extended in height to three storeys without prejudicing residential amenity or the integrity of the surrounding buildings and courtyard.

3. Palatine House

- 8.20 There are no current or anticipated development projects for this site but if Palatine House was redeveloped the site does have potential to accommodate a development block that follows the existing building line of the YOU and Molecular Imaging Centre and to match the height of these buildings (approximately four to five commercial-sized storeys -). As established by the adjacent Christie buildings, this would not adversely affect the character of Palatine Road. It will however be important to ensure that any development presents a strong and active frontage to Palatine Road and retains or matches the existing boundary treatment to the street: a low brick wall and tree planting.

4. Wilmslow Road

- 8.21 There is a site on the Wilmslow Road frontage that currently houses temporary accommodation. This has clear development potential and permanent buildings will be required to replace the existing temporary accommodation. The important issues here are presenting a strong frontage to the very visible main road frontage and preserving residential amenity and the character of the Conservation Area to the rear. A stand-alone building of up to three storeys will be appropriate, representing a step-down in height from the main Christie frontage to the two-storey shops on the northern side. The existing original boundary treatment of railings and a brick wall should be retained if possible.
- 8.22 Adjacent to this the main Christie frontage comprises the original 1930s brick-built range at 2 storeys, with an additional storey modern clad extension floors. At the southern end of the range is a modern four storey brick building with a similarly clad upper storey. This results in a frontage of 4 storeys at the southern end of this frontage, falling to 2 storeys adjacent to the temporary buildings at the northern end. This northern end has not been extended above the original brick frontage and it is felt that an extension to provide a third storey above this would be appropriate here. The extension could be designed as a better quality frontage than the modern cladding on the rest of this range. This would complement Withington Green and the surrounding frontages and would not compromise local character.

5. Mortuary and Estates Building

- 8.23 The mortuary and estates building is a relatively low rise building of two storeys in the interior of the main site (with the mortuary at ground level and the Estates Department offices on the first floor). An extension to provide an additional storey could be added without compromising the character of the surrounding area. This assessment has been based on analysis of views from Tatton Grove, which is within Withington Conservation Area.

6. Candleford Road

- 8.24 The Candleford Road site offers an opportunity for the Trust to provide ancillary non-clinical accommodation in the vicinity of the main Christie site. The existing Victorian building here has been refurbished for office accommodation and will be re-used by the Trust. If any new development occurs on this site it should respect the existing building lines (as shown on the Development Framework plan) and retain the mature trees and stone wall at the site boundary. Additional development or redevelopment proposals would be subject to first assessing the architectural and historic value of the building and the impact of the proposals upon its character. The site, and its operations, should continue to positively address Palatine Road and respect the amenity of surrounding residential and institutional neighbours (Including the single storey nursery to the north).



The Trust's property on Candleford Road

7. The Golden Lion

- 8.25 The former Golden Lion site provides an opportunity for the Trust's phasing and decant strategy to support redevelopment on other parts of the site. Any new development here, whether temporary or permanent, must respect the existing building and the amenity of surrounding residential properties. A domestic scale of two to two-and-a-half storeys would be appropriate. The existing mature trees on the edges of the site should be retained, unless they are shown to be in poor condition. A strong interface to the public street, with well maintained landscaping, will help to ensure that the new development integrates with its surroundings.



The Golden Lion site

Key Views and Sections

- 8.26 The images on the following pages are designed to show the potential street scenes and development form that would result from a maximum level of development that follows the principles described above. They are based on a computer-based massing model that has been specifically developed for this document. This indicates the maximum three-dimensional development envelopes in the context of existing buildings, shown in their appropriate position and scale.
- 8.27 Key views are taken of this model, shown alongside photographs of the existing views. Figure 8.2 shows where the views, and sections, are taken from. The views selected are important views that are experienced regularly by local residents and visitors and contribute to the townscape character of the area.

Figure 8.2: Key Views



3D Model Views (NB. these show development zone parameters and NOT actual building footprints)

Existing



Potential Development Envelopes



Key Views - Existing and Potential (see Figure 8.2)

1. Cotton Lane from Wilmslow Road



8.28 **Key characteristics** of the Wilmslow Road area comprise:

- Long continuous Christie frontages following the curve of the road.
- Development of between two and three-and-a-half storeys.
- Predominant use of brick.
- Buildings set back from the street to varying degrees, often with no boundary treatment to allow increased space for public realm activity.
- In general a wide road wide and open feel, with the curving road alignment allowing long views of the long Christie frontage.



8.29 Development Framework response:

- Buildings of larger massing to reflect original Christie buildings.
- Development of up to three commercial-sized storeys.
- Buildings set back from the street and enhanced public realm.
- Retention of Withington Green.
- Retention of brick wall and trees on Cotton Lane frontage.
- Elevations and materials to respond to existing materials palette.

2. Cotton Lane from Heyscroft Road



8.30 Key characteristics of Cotton Lane:

- The strong presence of the continuous brick wall in the street scene.
- The positive contribution of mature trees to the street scene.
- Traditional domestic scale post-war semi-detached houses.
- Larger scale Christie buildings set back from the street behind the wall.
- A dedicated separate vehicle access for Christie buildings from the western end of Cotton Lane behind the brick wall.



8.31 Development Framework response:

- Retention of brick wall on Cotton Lane and mature trees.
- Christie administration buildings (up to 3 residential-sized storeys closest to frontage) set back from the street behind the wall and trees.
- A dedicated separate vehicle access for the Christie buildings from the western end of Cotton Lane behind the brick wall and trees.
- Elevations and materials to respond to existing materials palette.

3. Cotton Lane from the East



8.32 Development Framework response:

- Retention of brick wall on Cotton Lane with some openings for pedestrian access.
- Retention of most mature trees with removal of a few trees (subject to the findings of a tree survey) to allow the new buildings to have a presence on the Cotton Lane frontage on this key corner.
- Christie administration buildings, up to 3 residential-sized storeys closest to frontage, set back behind wall and trees, (except on the above mentioned corner).
- Existing access retained for small staff car park and servicing.
- Elevations and materials to respond to existing materials palette.

4. Kinnaird Road from Wilmslow Road



8.33 The Kinnaird Road area has the following **key characteristics**:

- Two-and-a-half storey late Victorian large semi-detached houses within spacious plots.
- A frontage on both sides of streets consisting of 1m high brick walls with planting behind.
- A strong boulevard of trees on the Christie's Kinnaird Road frontage.
- A changing relationship between the Christie site and fronts/backs of adjacent housing.



8.34 Development Framework response:

- Potential for new MCRC Phase 2 development (up to 3 commercial-sized storeys).
- This development would be set back considerably from the street, behind an attractively landscaped public plaza.
- Retention and enhancement of existing wall and boulevard of trees.
- Removal of existing single storey-administration blocks.
- The Maggie's Centre will terminate this street vista: a domestically proportioned but iconic building designed by a signature architect.
- Pedestrian access to the MCRC Phase 2 and Maggie's Centre will be provided from the street but vehicle access will be via the existing site access road from Cotton Lane.
- Elevations and materials to respond to existing materials palette.

5. Oak Road from Wilmslow Road



8.35 Key characteristics of Oak Road:

- Christie buildings of around 4 commercial storeys with large footprints and a set-back of around 6m from the street.
- Residential buildings between two and two-and-a-half storeys, in terraced form, with a set-back of around 5m from the street.
- A strong landscape edge to the street on both sides with mature trees softening the street scene.
- Traditional materials on the southern side of the street and contemporary materials on the northern side of the street.



8.36 Development Framework response:

- New 4-storey development behind the existing Christie development on Oak Road, at the same height and following the same building line as the existing buildings.
- Retained and enhanced trees and planting along the Oak Road frontage.
- Potential for a three-storey 'bookend' development to define the Wilmslow Road/Oak Road corner in place of the existing substation.
- Elevations and materials to respond to existing materials palette.

6. Oak Road from Palatine Road



8.37 Key characteristics – see view 5.



8.38 Development Framework response – see view 5.

7. Palatine Road, looking northwards



8.39 Key characteristics of the Palatine Road area:

- A strong established building line (of around 8m and then 18m set-back from the back-of-pavement on the Christie side).
- Planting to the front of properties, including significant numbers of mature trees which creates a boulevard along the long straight road.
- A consistent presence of low stone walls to define the front boundary of properties.
- Buildings of significant massing, between two-and-a-half to four storeys with large footprints.
- The presence of both traditional Victorian materials (i.e. brick and slate) and contemporary materials (glazing, cladding panels, terracotta).



8.40 Development Framework Response:

- Potential for redevelopment of the Palatine House site.
- New development to follow the established 8m building line established by the existing Christie buildings (and the forthcoming YOU).
- Retention of mature tree planning on frontage (or removal and replacement) and retention of stone wall.
- Elevations and materials to respond to existing materials palette.

8. Palatine Road, looking southwards



8.41 Key characteristics – see view 5.



8.47 Development Framework response – see view 5.

Key Site Sections of Development Parameters

Figure 8.3: Site Sections Key Plan

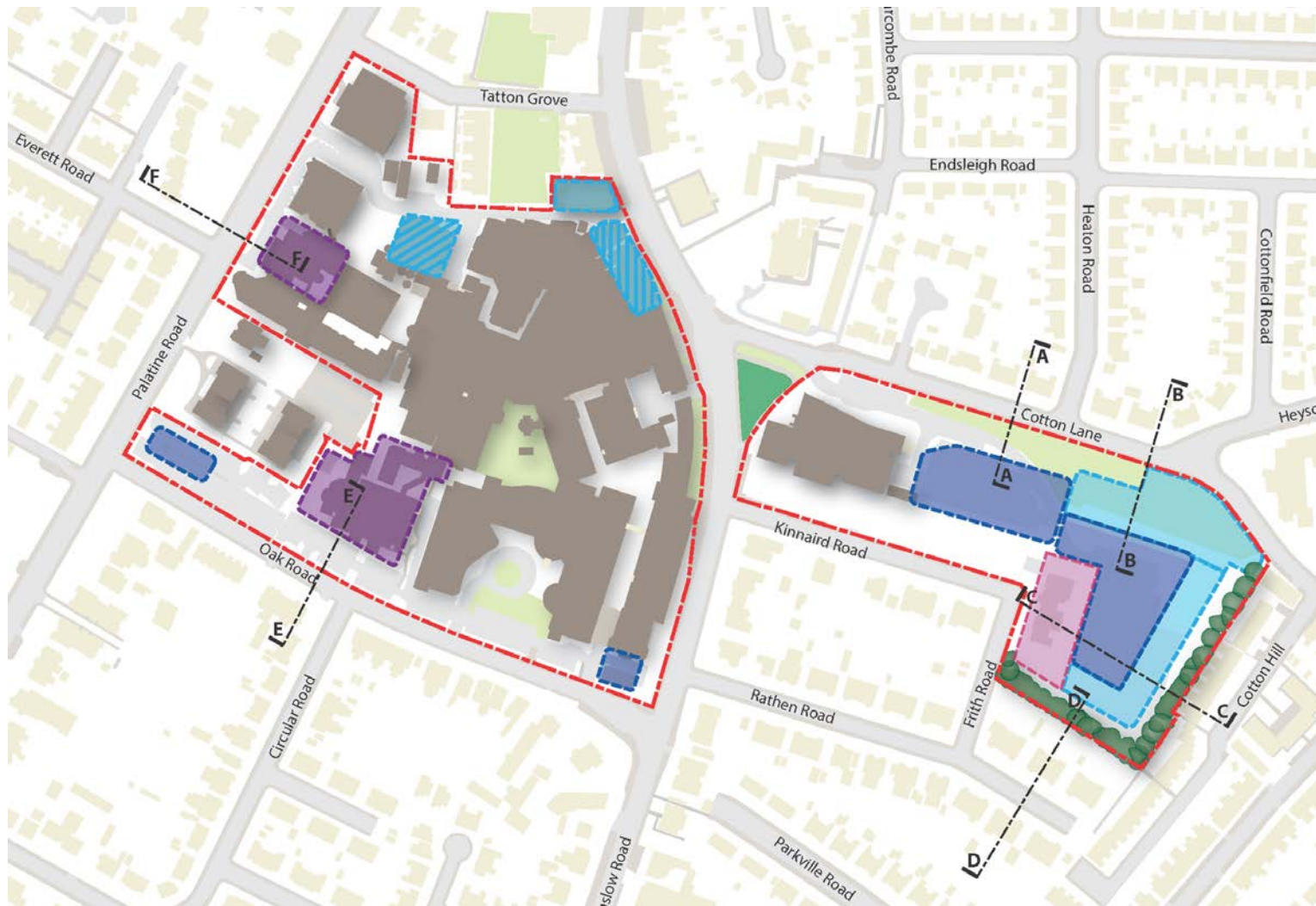
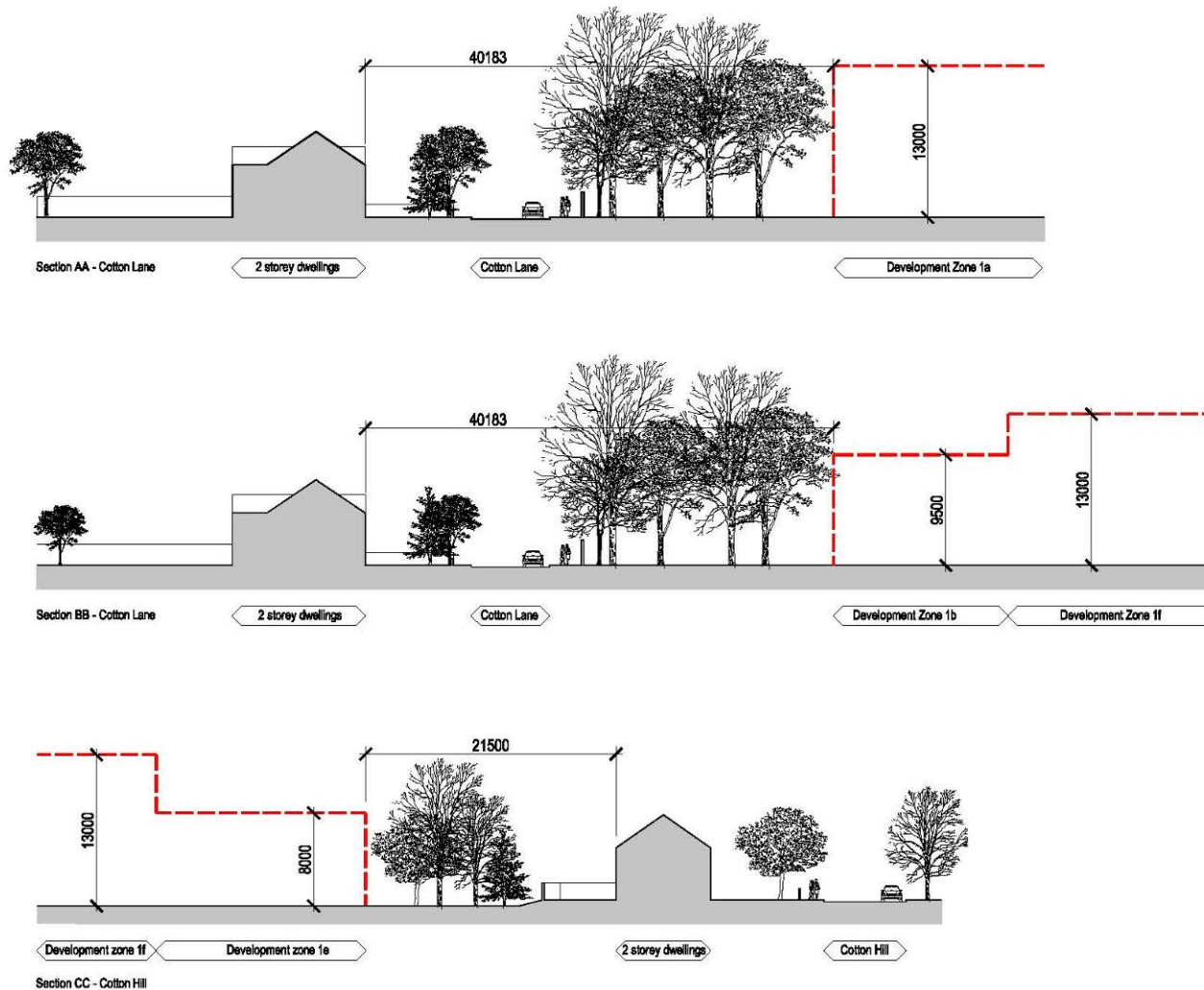


Figure 8.4: Potential Site Sections 1 (note – dimensions are approximate)



NOTES:
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drawing originated by: LF date: 16/11/12
 revisions: drawn/reviewed date
 REV A LF/JP 22/11/12
 Section CC: amended storey heights and ground levels.

PRELIMINARY

Christie Strategic Hospital Report

Street Sections
 Sheet 01

Scale: 1:500 @ A4
 Reviewed by: -

Job drawing rev
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taylor young ty

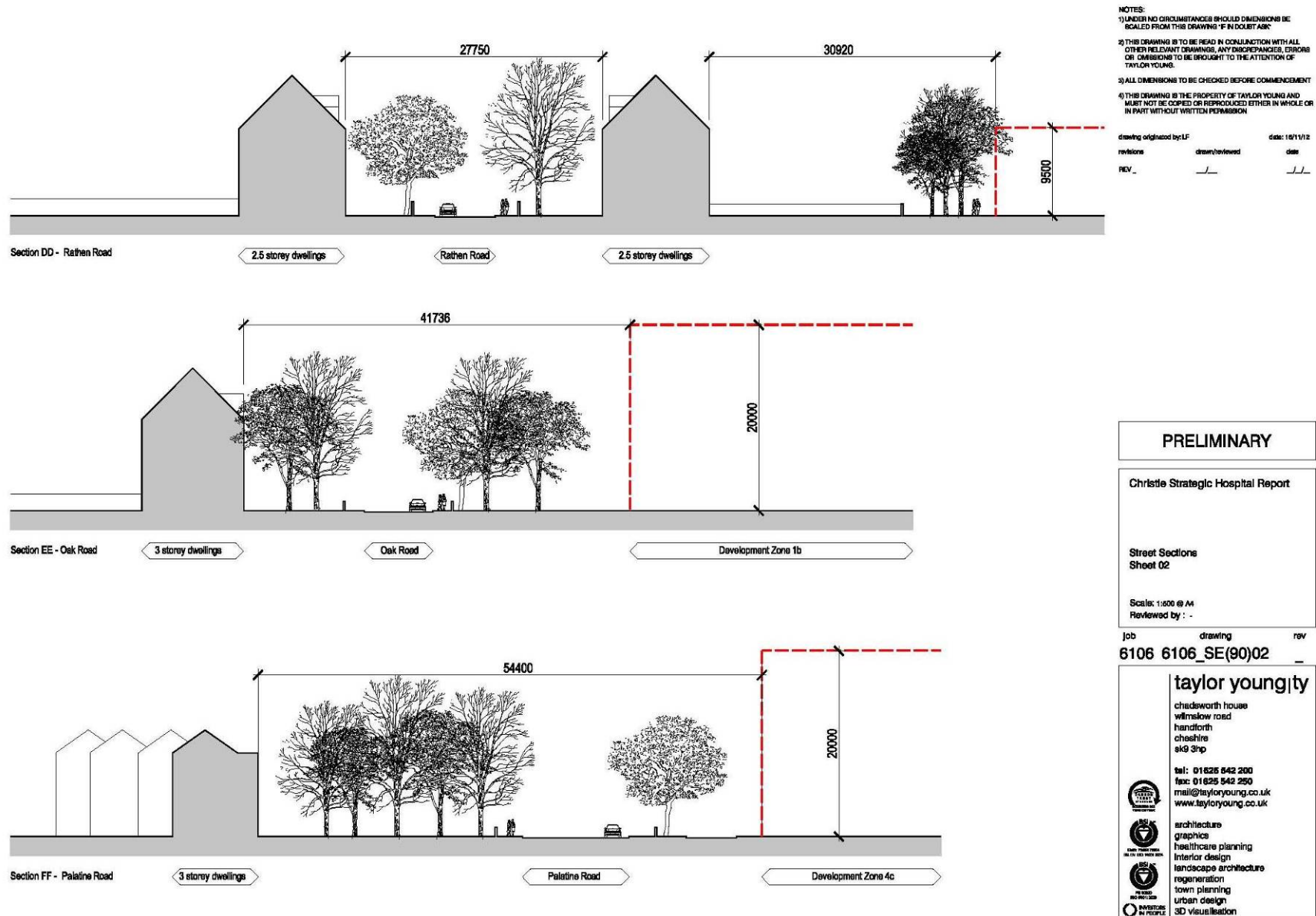
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architecture
 graphics
 healthcare planning
 interior design
 landscape architecture
 regeneration
 town planning
 urban design
 3D visualisation

Figure 8.5: Potential Site Sections 2 (note – dimensions are approximate)



9 THE CHRISTIE TRANSPORT PLAN

Development of the Green Travel Plan

9.1 The Christie Transport Strategy

The Christie Transport Strategy is to reduce the overall number of single occupancy vehicles travelling to the Withington site to 40% by 2030, to promote sustainable modes of travel and to deliver a car parking solution which results in a reduction of on street parking in the vicinity of the hospital.

Actions include:-

- 1. Implementation of a park and ride scheme for staff car parking**
- 2. To implement the Green Travel Plan**
- 3. On site car parking to be at ground level or low rise i.e. ground floor plus 2 additional levels as a maximum (3 levels of parking)**
- 4. Potential to move certain back office activities and associated car parking off the Withington site**

9.2 Initiatives to deliver the Transport Strategy

The Christie have included funding within their 5yr Capital Plan to provide support to their Transport Strategy and its associated initiatives.

The Christie have appointed a Travel Plan Coordinator who plays a key role in the implementation of the travel plan. He also sits on the Travel Choices Employers' Forum, - a forum for Manchester businesses members who are ambassadors for promoting sustainable travel within their organisations.

A structured Communication Plan will be deployed to keep staff and visitors informed of the plans, initiatives and campaigns which are designed to meet the aims of the transport strategy. This includes:-

- The monthly Team Brief open to all staff and chaired by the CEO where key initiatives and news is communicated.
- Green Travel Plan initiatives regularly updated on the intranet
- AECOM/TfGM are working with the TravelPlan Co-ordinator to hold a Personal Travel Planning event to re – launch the transport strategy (This is set for 9 July 2014)
- Articles and news in the Christie magazine “Chinwag”
- Events and information boards to communicate planned change

Attached in Appendix 3 is The Christie's latest revised Green Travel Plan detailing the initiatives planned and employed to achieve the modal shift targets.

A summary of The Christie initiatives are listed below:-

The Trust proposes to encourage sustainable transport use by prioritising, in this order, cycling and walking, tram/ bus/train, motorcycle, car sharing; and will discourage single occupancy car (or other motorised vehicle) access for staff.

A named dedicated member of staff will oversee this work and specific targets will be owned by identified members of staff.

- interest free season ticket loans to all staff
- discounted weekly, monthly and annual Stagecoach season
- 10% discount on annual Metrolink season ticket i
- regular promotional events, including an annual Health and Wellbeing day.
- 50% car parking discount for car sharing is in operation
- provide new staff with a comprehensive information pack of the sustainable travel options .
- engage travel advisors to work with staff to ensure that they are fully aware of the travel options offer of cycle training to staff;
- the bike to work scheme offering employees a tax-free benefit.
- ensuring staff have access to cycle facilities and twice-yearly free 'bike doctors';
- reviews of the car parking permit system and pricing strategy
- continually review car parking , permit and eligibility
- locate allocated staff parking spaces away from the main building entrances;
- locate cycle parking in a prominent location close to the main building entrances;
- Work closely with Metrolink and bus and rail operators, to discuss opportunities for further and ongoing bespoke ticketing and marketing materials for staff

- discussions are in place with First Bus Manchester on travel costs for employees and possible future smarter ticketing
- an interactive travel plan is currently under development with access via the intranet.

9.3 Car Parking Plans

The Christie are aiming to achieve the modal shift detailed in the Green Travel Plan to reduce parking on the Christie site. However, this document recognises that there will be a continuing need to provide car parking at the hospital for both staff , patients and visitors.

9.4 Short, Medium and Long Term car parking plans

Short Term 2014 – 2016

- To progress the aims of the Green Travel Plan and update where required in consultation with MCC
- Implementation of the Resident Car Parking Zone by MCC. As previously illustrated in this document the Trust is funding the implementation of a parking scheme on streets surrounding the Christie. A time limited waiting restriction will apply allowing patients to park for short periods but prevent staff from parking. The extent of the zone is shown in fig. 5.2
- To provide new surface level car parking of some 82spaces on Christie land to the rear of Blocks A, B, and C (between the buildings and the boundary wall on Cotton Lane), on the Kinnaird Road site.
- To review eligibility for staff parking permits
- Provided dedicated patient only parking spaces on the Christie side of Kinnaird road

- To utilise the Golden Lion site as a site for temporary car parking during development activities

Medium Term 2016 – 2018

- To progress the aims of the Green Travel Plan and update where required in consultation with MCC
- To progress a park and ride scheme. (A Budget has been allocated by the Trust to acquire or lease land in the vicinity of the hospital to provide satellite car parking (park and ride) for majority of Trust staff members).
- Low rise car parking on part of the Kinnaird Road site i.e. ground floor plus 2 additional levels as a maximum (3 levels of parking)
- To utilise the Golden Lion site as a site for temporary car parking during development activities

Longer Term

- To progress the aims of the Green Travel Plan and update where required in consultation with MCC
- Park and Ride for Staff
- The Trust will consider the potential for moving certain back of house operations off site if suitable and an affordable alternative accommodation can be sourced without adversely affecting the operation of the hospital. This will reduce parking on and around the Withington site.
- To consider the use for the Golden Lion site when no longer required for car parking. The trust will consider a development for the benefit of patients and the Trust.

Reporting progress

The Christie will collect and regularly report the following information

- Staff modal share
- Staff aggregate postcode data
- Take up of policy incentives e.g. tax free bikes scheme, and interest free loans
- Use of infrastructure e.g. cycle parking, and car share car park
- Car Park utilisation

This will feed into annual progress reports and action plans for the Green Travel Plan for submission to Manchester City Council and to commit to using reasonable endeavours to deliver the targets set out in the Plan.

To ensure that specific dates are set against this; the first report to be produced 12 months from the date of the first use of the site and produced annually from this point.

9.5 Further actions are noted here in respect of the Christie's plans

- Senior management responsibility for the Green Travel Plan, including the overall delivery of the transport strategy, will be at Board Level.
- Greater emphasis will be placed on car sharing and access to a car sharing data base.
- Cycle user groups and buddy schemes can be effective. This will continue to be promoted within the Trust working with enthusiasts to co ordinate and implement this.
- Travel behaviour will not be presented as an all or nothing initiative. A complete change in travel behaviour can be a real

disincentive but presented as a day or week or even fortnight event can be acceptable and from small beginnings more significant change can follow. For example United Utilities headquarters in Warrington operate a 4 days a week parking system with staff using sustainable modes one day a week.

Future Parking Framework

- 9.6 The analysis in this report clearly presents a number of challenges for the Trust in terms of managing transport patterns and resulting parking demand, if the Christie's important development aspirations are not be compromised.
- 9.7 The first objective must be to continue to change the behaviour of staff and promote more travel by sustainable means and car sharing. Significant progress on this has already been made by the Trust since 2005 – reducing the percentage of staff who drive to work from 71% to 63%.
- 9.8 The analysis of current parking demand and provision reveals a significant shortfall in supply of staff parking in comparison to demand which needs to be addressed. This is corroborated by the large numbers of staff that are parking on surrounding streets (around **800** staff per day).When the resident parking scheme is implemented as currently proposed this will encourage staff to consider alternative modes of transport to get to work
- 9.9 These changes in travel behaviour will not happen overnight; based on existing travel patterns the outstanding demand will be **840** spaces. Even when the challenging Green Travel Plan targets are

applied there would still be an outstanding demand of **465-570** spaces.

- 9.10 There is then the need to consider the additional staff parking demand that will result from the committed, emerged and anticipated new developments – which are of major importance for both the Trust and the City in terms of both healthcare treatment and the local economy. Based on current estimates of the number of additional jobs this is likely to result in a total outstanding demand of **665** spaces
- 9.11 Patient and visitor parking must also be factored into considerations of supply and demand. This is a less major issue numerically than that of staff parking. Nevertheless patient/visitor car parks are currently at capacity and surveys indicate that around 120 patients/visitors are parking on-street every day. Future demand of projected new development equates to an additional 16 spaces for the Proton Centre.
- 9.12 There is a physical maximum to the extent of parking that can be provided on the Christie site – this is defined by the parameters shown within the Development Framework (Figure 8.1). These parameters are based on detailed analysis and need to be maintained to ensure that there is no detrimental impact to local character and residential amenity. Furthermore, development within these zones is likely to displace a significant number of existing spaces, which would then need to be added to the additional parking demand.
- 9.13 It therefore appears unlikely that sufficient parking can be provided on the Christie site to fully meet future parking demand on site, even when challenging Travel Plan targets are applied. Clearly there are significant challenges here and a creative solution must be applied. **Table 9.1** below sets out three options to address this deficit. These options consider potential off-site park and ride options and a range

of on site opportunities. It is worth noting that all options have assumed that the Trust will seek to meet the most challenging Travel Plan targets that are achievable

- 9.15. A fourth option which included a substantial multi-storey car park on the Kinnaird Road site has been rejected by the Trust.
- 9.16 It is also recommended that the City Council continues to review the detail of the restricted parking scheme (in terms of time controls if not spatially) in light of the implications that this is likely to have. There are areas within the controlled zone that could be used for some parking without compromising residential amenity – for example the spaces alongside the wall on Cotton Lane, which are not in front of houses. The needs of all user groups should be considered when developing the resident parking scheme.
- 9.17 The Christie intend to implement option 3 in table 9.1 subject to all necessary approvals being secured.

Table 9.1: Strategic Car Parking Options

Option	Description	Advantages	Disadvantages
1	Retain current surface parking layout and use balance of site for development parcels. Implement enhanced travel plan	Low visual impact on surrounding residential area. Low cost solution	Limits the development capacity significantly Doesn't address car parking shortfall or pressure for parking in surrounding areas
2	Create development parcels with surface parking retained at significantly reduced scale Establish Park & Ride off site for majority of current and future demand Implement enhanced travel plan	Low visual impact on surrounding residential area, and reduced on site traffic impacts. Addresses overall parking requirements	Limits development capacity Dependent on implementing Park & Ride elsewhere successfully (site / development / use)
3	Create development parcels including a limited scale MSCP to maintain current levels of parking Establish Park & Ride off site for balance of demand Implement enhanced travel plan	Retains more parking on the Christie site than option 2. Provides more development opportunities than Options 1 or 2 Addresses overall parking requirements	Dependent on implementing Park & Ride elsewhere successfully (site / development / use) External perceptions of additional site development compared to Option 2.

10 NEXT STEPS

10.1 This Strategic Planning Framework makes several important contributions. It has:

- **Explained the importance of the Christie and the Trust's development aspiration to the people of Manchester – in terms of treatment and also economic benefits including jobs.**
- **Set out the Trust's development commitments and aspirations for the next 15 years. This will be subject to regular review with scope to update this document in response.**
- **Established appropriate scale and location parameters for development that could be positively accommodated without compromising local character and residential amenity.**
- **Set out recommended measures to make significant improvements to the Travel Plan and encourage further modal shift to challenging but achievable levels.**
- **Analysed patterns of current and projected parking supply and demand.**

10.2 The parking analysis has identified a significant immediate excess demand which will become even more apparent when the resident parking scheme is implemented. This parking deficit will increase to 720-840 spaces, when projected developments are completed, and assuming that challenging Travel Plan targets are met. A failure to address this excess demand has the potential to limit the Trust's important development aspirations.

10.3 This report sets out a number of potential strategic options to address this parking issue. The Trust are currently pursuing these options and will resolve the long-term parking requirement. The preferred

approach will be discussed and agreed with Manchester City Council and shared with the Neighbourhood Forum.

10.4 This Planning Framework has now been formally signed off by the Trust and has been the subject of public consultation by the City Council, following which further amendments have been made. Following further endorsement by the City Council it will then become a material consideration against which planning applications will be assessed. This will give it a real status as useable and useful document to steer subsequent planning proposals to delivery and to help alleviate resident's concerns.

10.5 To remain useful the Framework will be the subject of regular monitoring and review. The Trust will update the Framework as development is implemented and their commitments and development aspirations change – which is inevitable in the context of the ever-changing healthcare treatment and research. This review and update process will be jointly undertaken by the Trust and the City Council and will be the subject of future consultation with the Neighbourhood Forum.





APPENDIX 1. Supporting Transport Data 2012



Christie Hospital

Parking Data



On Site Parking

The Trust survey levels of car parking in the two main staff car parks every day. The results of the surveys from September to December 2012 are shown in the attached graph labelled “Staff Car Park Daily Survey”. The graph shows the car parks are generally relatively full. As explained in the main text this period coincided with works to the Kinnaird Road car park and this was almost certainly putting some staff off from parking on site.

Patient parking is not generally surveyed. However to assist the study the Trust undertook surveys of patient parking during two weeks in November 2012. The results of these surveys are included in the attached Table labelled “Patient Parking”. As explained in the main text the patient car parks are full on a daily basis.

On Street Parking

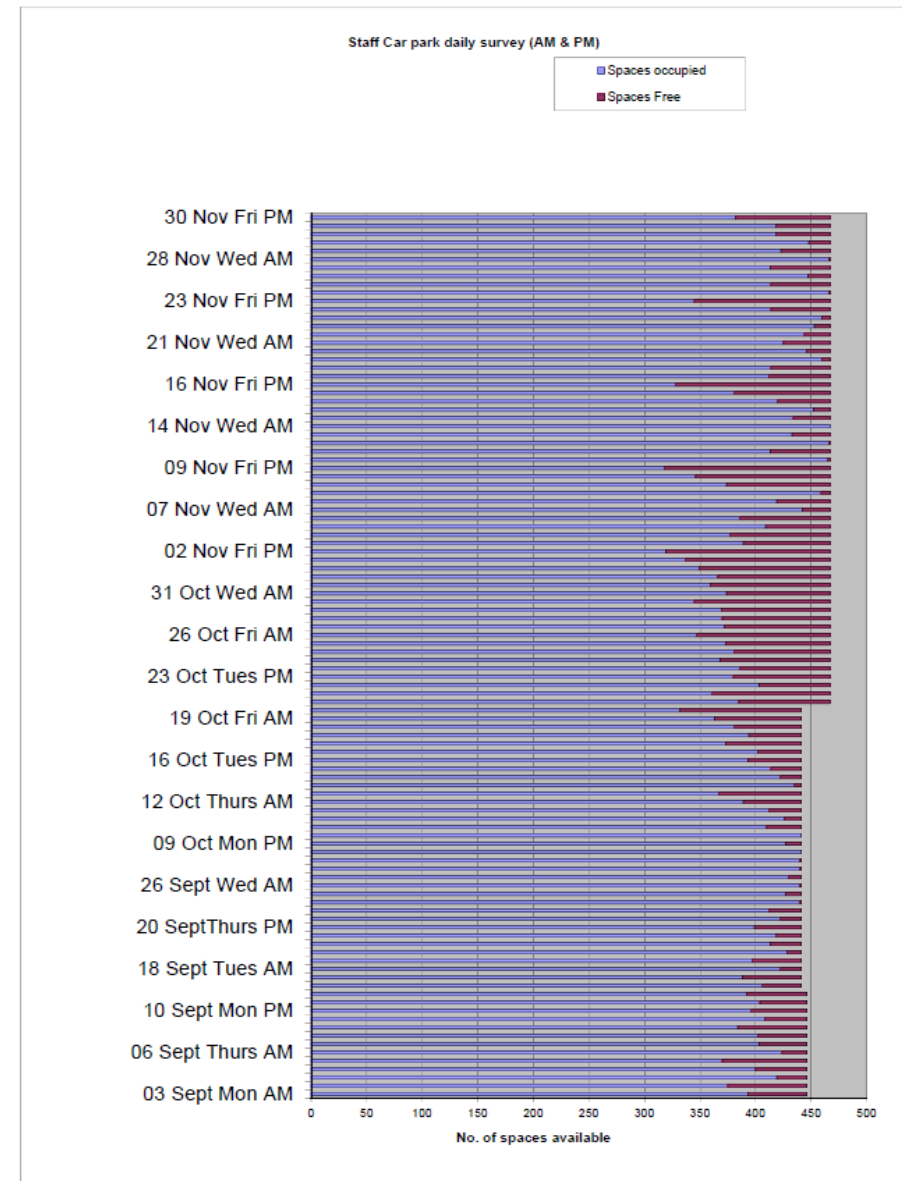
An independent survey company was commissioned to undertake an extensive survey of on street parking in the week commencing 26th November 2013. Parking beat surveys were undertaken every 2 hours during the working day which recorded the registration plates of all vehicles parked. From this data the survey company identified the duration of stay of each vehicle.

A survey was also undertaken overnight on Monday 26th November which recorded the registration plates of all vehicles parking overnight. Based on the assumption that the overnight parkers were resident’s vehicles the day time data was analysed to determine the number of vehicles parking on street which were resident’s cars.

A summary of the results of these surveys shown in the attached Table labelled “On Street Parking Summary Table.” The results for each day by street are also included.

There are extensive numbers of non-residents cars who park on street for most of the day. Practically all of these will be associated with the Christie Staff.

There are also a significant number of people parking on street for relatively short periods of time. We believe most of these will be Christie patients although unlike the long stay parkers there are likely to be a number who are not associate with the Christie. To provide a robust analysis of Christies’ parking demand it has been assumed all the short stay vehicles are associated with the Christie.



[Patients](#)

Patient Parking							
Date	Time	Kinnaird Road Car Park		Oak Road Car Park			
		Main Area	Disabled	Private Patients	Radiology	Main Area	Disabled
05-Nov-12	9am	29	Full	18	13	42	5
05-Nov-12	11am	Full	Full	18	Full	Full	Full
05-Nov-12	1pm	Full	Full	9	2	Full	Full
05-Nov-12	3pm	8	Full	7	Full	11	Full
05-Nov-12	4pm	8	Full	8	Full	1	Full
06-Nov-12	9am	32	Full	22	Full	38	Full
06-Nov-12	11am	Full	Full	17	Full	Full	Full
06-Nov-12	2pm	1	Full	8	3	2	4
06-Nov-12	4pm	23	1	13	8	30	2
07-Nov-12	9am	29	Full	19	2	40	Full
07-Nov-12	12pm	2	Full	2	3	Full	Full
07-Nov-12	2pm	Full	Full	6	Full	Full	Full
07-Nov-12	4pm	34	1	8	1	8	1
08-Nov-12	9am	17	Full	19	4	44	3
08-Nov-12	12pm	Full	Full	6	Full	Full	Full
08-Nov-12	2pm	Full	Full	7	Full	Full	Full
08-Nov-12	4pm	20	Full	7	2	14	Full
09-Nov-12	9am	43	Full	17	15	44	Full
09-Nov-12	12pm	17	Full	7	13	6	Full
09-Nov-12	2pm	3	Full	8	10	4	Full
12-Nov-12	9am	25	Full	24	Full	38	Full
12-Nov-12	11am	Full	Full	2	2	Full	Full
12-Nov-12	3pm	1	Full	6	Full	Full	Full
12-Nov-12	4pm	Full	Full	6	4	9	1
13-Nov-12	9am	29	Full	20	3	43	2
13-Nov-12	11am	Full	Full	8	Full	Full	Full
13-Nov-12	3pm	19	Full	12	Full	8	Full
13-Nov-12	4pm	20	Full	13	1	28	Full
14-Nov-12	9am	24	Full	18	Full	32	Full
14-Nov-12	11am	Full	Full	4	Full	Full	Full
14-Nov-12	3pm	8	Full	4	Full	5	Full
14-Nov-12	4pm	8	Full	6	1	4	2
15-Nov-12	9am	25	Full	17	Full	40	1
15-Nov-12	11am	Full	Full	11	Full	Full	Full
15-Nov-12	1pm	Full	Full	5	Full	Full	Full
15-Nov-12	3pm	10	Full	3	Full	Full	Full
15-Nov-12	4pm	24	Full	3	1	8	Full
15-Nov-12	9am	22	Full	20	Full	41	3
16-Nov-12	11am	Full	Full	12	Full	Full	Full
16-Nov-12	1pm	Full	Full	4	Full	6	Full
16-Nov-12	3pm	8	Full	4	2	11	2
16-Nov-12	4pm	10	1	7	6	16	2

On Street Parking Summary Table

	09:00				11:00				13:00				15:00			
	Residents	Visitor - Long Stay	Visitor - Short Stay	Total Visitor	Residents	Visitor - Long Stay	Visitor - Short Stay	Total Visitor	Residents	Visitor - Long Stay	Visitor - Short Stay	Total Visitor	Residents	Visitor - Long Stay	Visitor - Short Stay	Total Visitor
Monday	309	763	109	872	268	792	105	897	252	769	103	872	224	671	98	769
Tuesday	270	751	117	868	239	816	128	944	228	789	122	911	229	708	121	827
Wednesday	268	785	119	904	213	807	129	936	199	805	126	931	188	761	141	902
Thursday	273	750	102	852	239	807	114	921	237	794	113	907	236	722	146	868

Code	Street	Overnight 01:00 - 05:00	Monday 26/11/2012											
			09:00			11:00			13:00			15:00		
			Residents	Victor - Long Stay	Victor - Short Stay	Residents	Victor - Long Stay	Victor - Short Stay	Residents	Victor - Long Stay	Victor - Short Stay	Residents	Victor - Long Stay	Victor - Short Stay
1	ALAN RD	25	9	13	2	7	17	1	7	18	3	7	17	2
2	ALDBOROUGH CLOSE	0	0	8	1	0	8	2	0	7	0	0	5	0
3	ALVERSTONE RD	3	2	7	0	2	7	0	2	7	0	2	6	0
4	ARNFIELD RD	46	20	48	2	17	52	3	15	51	1	15	47	4
5	BURTON AVENUE	10	4	7	0	4	8	1	4	7	0	2	2	0
6	CENTRAL RD	21	10	5	0	9	5	1	6	3	0	5	4	0
7	CHATTERTON CLOSE	2	1	2	1	1	4	1	1	4	0	1	4	0
8	CIRCULAR RD	24	6	20	10	3	21	15	4	20	13	3	17	7
9	COTTON HILL	22	6	20	2	3	19	0	5	19	0	4	16	1
10	COTTON LANE	11	4	34	4	0	30	8	0	29	11	0	28	4
11	COTTONFIELD RD	13	5	19	0	5	21	0	5	21	0	5	21	0
12	CROMER AVENUE	18	10	9	0	9	10	0	7	9	2	5	6	2
13	DANESMOOR ROAD	39	9	30	2	8	28	1	8	28	0	8	23	0
14	DELAINE RD	29	8	23	2	8	24	1	7	23	0	8	20	0
15	ENDSLEIGH RD	3	3	5	0	1	6	0	1	6	0	1	6	0
16	EVERTT ROAD	49	11	46	6	8	48	6	8	45	5	8	43	8
17	FERNDENE RD	17	6	36	2	5	35	1	6	34	1	6	29	1
18	FERNGATE DRIVE	8	5	2	0	5	2	1	5	2	1	2	1	1
19	FRANCIS RD	16	6	14	0	4	14	0	4	15	0	3	13	4
20	FRITH RD	0	1	19	7	1	19	6	1	22	4	1	18	4
21	GORDON PLACE	11	6	6	0	6	6	0	6	6	0	5	3	0
22	GOULDEN RD	22	10	19	0	10	20	1	10	20	1	8	14	1
23	HAGUE RD	18	6	4	1	4	4	2	3	4	0	2	3	0
24	HARCOMBE RD	5	3	11	3	3	10	1	3	10	2	3	8	0
25	HEATON RD	18	14	24	5	13	24	3	11	22	5	11	22	6
26	HENSWOOD RD	57	19	11	0	20	14	0	19	17	0	17	13	0
27	HEYSCROFT ROAD	25	1	10	4	1	10	2	1	8	2	1	10	2
28	KINNAIRD RD	5	1	18	2	0	21	1	0	13	4	1	16	4
29	LYNWAY DRIVE	0	0	0	0	0	0	0	0	0	0	0	0	0
30	MARDALE AVENUE	8	1	11	0	1	11	0	1	11	0	1	11	0
31	MARRIOTT STREET	7	4	5	0	4	5	0	4	5	0	4	5	0
32	NESTON AVENUE	8	5	4	1	4	5	2	2	3	3	2	2	2
33	OAK RD	15	4	6	18	1	10	24	1	10	24	0	8	17
34	PALATINE AVENUE	13	2	10	0	2	10	0	2	10	0	2	7	0
35	PALATINE CRESCENT	10	3	1	0	3	1	0	2	1	0	2	1	0
36	PALATINE RD	6	0	0	4	0	1	5	0	6	10	0	5	9
37	PARKGATE AVENUE	7	3	10	0	3	11	0	2	11	0	1	8	0
38	PARKVILLE ROAD - KINGSLEA TO PARRS WOOD ROAD	8	2	8	3	2	8	1	2	8	0	2	7	0
39	PARKVILLE ROAD - KINGSLEA TO WILMSLOW ROAD	24	6	71	4	5	68	3	5	66	1	5	66	1
40	PRIDMOUTH RD	6	3	17	0	2	18	0	2	18	0	2	17	0
41	RATHEN RD	11	4	15	5	5	16	3	4	15	5	2	7	6
42	REDCAR AVENUE	18	7	13	2	7	13	1	6	15	0	2	11	1
43	SANDILEIGH AVENUE	42	14	30	0	14	27	0	13	27	0	13	22	0
44	TATTON GROVE	10	4	17	5	3	21	2	3	20	3	1	17	5
45	TENBY AVENUE	25	9	11	3	8	15	1	9	15	0	6	12	1
46	WENSLEY DRIVE	7	0	8	0	0	8	0	0	8	0	0	6	0
47	WILMSLOW ROAD	0	1	23	3	1	23	2	1	21	0	1	14	0
48	WOLSELEY PLACE	11	5	10	0	5	10	0	4	10	0	3	8	0
50	BRIDGELEA ROAD	12	9	11	1	7	11	1	7	10	0	8	9	2
51	LONGTON AVENUE	34	18	5	2	17	5	0	14	4	0	15	3	3
52	HOLLY ROYDE CLOSE	39	18	1	1	17	0	2	18	1	2	18	1	0
53	CANDLEFORD ROAD	1	1	6	1	0	8	0	1	4	0	0	9	0

309 763 109 268 792 105 252 769 103 224 671 98

Code	Street	Overnight 01:00 - 05:00	Tuesday 27/11/2012											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN RD	25	10	13	2	7	20	3	6	20	3	7	18	3
2	ALDBOROUGH CLOSE	0	0	9	0	0	9	0	0	11	0	0	4	1
3	ALVERSTONE RD	3	2	8	0	1	9	0	1	8	0	1	9	0
4	ARNFIELD RD	46	13	45	4	10	53	2	10	50	6	10	51	5
5	BURTON AVENUE	10	3	7	0	3	8	1	3	7	1	2	5	0
6	CENTRAL RD	21	8	2	1	6	2	2	6	2	1	6	1	3
7	CHATTERTON CLOSE	2	1	2	0	1	2	0	1	2	0	0	2	0
8	CIRCULAR RD	24	3	26	13	2	29	11	3	34	6	3	26	14
9	COTTON HILL	22	3	22	1	4	23	1	4	23	1	2	22	2
10	COTTON LANE	11	4	33	3	4	34	1	5	32	3	4	22	10
11	COTTONFIELD RD	13	3	20	1	3	21	1	3	20	0	3	18	1
12	CROMER AVENUE	18	8	11	0	7	13	0	9	13	0	9	14	0
13	DANESMOOR ROAD	39	12	21	1	11	20	5	9	21	5	7	16	5
14	DELAINE RD	29	6	21	0	6	23	2	7	24	0	7	22	0
15	ENDSLEIGH RD	3	2	6	0	2	7	0	2	7	0	1	6	1
16	EVERTT ROAD	49	10	48	2	9	49	4	7	49	8	9	44	6
17	FERNDENE RD	17	7	32	2	7	33	1	6	35	1	6	33	1
18	FERNGATE DRIVE	8	3	4	1	1	5	0	1	5	0	2	4	1
19	FRANCIS RD	16	4	9	2	4	12	2	2	12	2	1	11	4
20	FRITH RD	0	0	24	2	0	25	1	0	24	1	0	21	1
21	GORDON PLACE	11	7	2	2	5	3	1	5	3	1	5	3	1
22	GOULDEN RD	22	8	14	3	9	18	5	8	16	4	7	15	0
23	HAGUE RD	18	1	8	1	1	8	2	1	7	2	1	6	1
24	HARCOMBE RD	5	1	13	0	1	13	1	1	13	1	1	13	0
25	HEATON RD	18	10	31	2	11	32	2	10	32	2	10	33	1
26	HENSWOOD RD	57	18	13	0	18	13	0	18	13	0	15	13	1
27	HEYSCROFT ROAD	25	3	13	1	1	13	0	3	13	0	2	12	0
28	KINNAIRD RD	5	2	18	0	3	18	0	2	17	1	2	13	1
29	LYNWAY DRIVE	0	0	0	0	0	0	0	0	0	0	0	0	0
30	MARDALE AVENUE	8	3	10	0	2	9	1	2	9	0	2	9	0
31	MARRIOTT STREET	7	3	7	3	2	6	0	2	4	5	4	3	4
32	NESTON AVENUE	8	4	7	2	4	7	3	3	9	0	3	9	0
33	OAK RD	15	2	8	21	1	8	26	1	7	22	0	11	21
34	PALATINE AVENUE	13	4	11	0	3	11	1	2	10	2	3	10	1
35	PALATINE CRESCENT	10	5	0	1	4	0	0	3	0	0	3	0	0
36	PALATINE RD	6	1	5	3	1	11	9	0	14	3	0	14	6
37	PARKGATE AVENUE	7	3	9	0	3	9	0	3	9	0	2	9	0
38	PARKVILLE ROAD - KINGSLEA TO PARRS WOOD ROAD	8	2	2	1	3	10	3	2	11	4	2	7	3
39	PARKVILLE ROAD - KINGSLEA TO WILMSLOW ROAD	24	5	67	5	5	70	5	4	68	4	4	59	3
40	PRIDMOUTH RD	6	2	15	0	3	15	0	2	14	3	2	14	2
41	RATHEN RD	11	5	18	2	5	16	2	5	16	1	5	12	2
42	REDCAR AVENUE	18	7	14	0	6	16	0	5	16	0	5	15	0
43	SANDILEIGH AVENUE	42	14	23	7	13	22	10	11	20	6	11	18	5
44	TATTON GROVE	10	3	16	5	2	16	5	3	15	4	3	16	5
45	TENBY AVENUE	25	11	15	0	8	17	0	8	16	0	13	12	0
46	WENSLEY DRIVE	7	3	4	3	2	5	1	2	4	0	2	4	0
47	WILMSLOW ROAD	0	0	24	15	0	21	11	0	14	10	0	4	2
48	WOLSELEY PLACE	11	2	5	1	3	5	2	2	3	2	2	2	2
50	BRIDGELEA ROAD	12	7	5	0	6	11	0	6	6	2	7	5	1
51	LONGTON AVENUE	34	14	5	2	12	8	0	12	7	0	14	7	1
52	HOLLY ROYDE CLOSE	39	17	0	1	14	0	1	16	0	5	19	0	0
53	CANDLEFORD ROAD	1	1	6	1	0	8	0	1	4	0	0	9	0
			270	751	117	239	816	128	228	789	122	229	706	121

Code	Street	Overnight 01:00 - 05:00	Wednesday 28/11/2012											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN RD	25	7	13	3	3	13	0	3	13	0	3	13	0
2	ALDBOROUGH CLOSE	0	1	9	0	0	9	0	0	10	0	0	8	0
3	ALVERSTONE RD	3	2	8	0	1	8	0	1	8	0	1	8	0
4	ARNFIELD RD	46	13	51	2	7	56	7	6	54	4	6	56	5
5	BURTON AVENUE	10	2	9	0	1	8	0	1	10	0	1	8	0
6	CENTRAL RD	21	8	4	3	7	4	0	7	4	0	7	4	0
7	CHATTERTON CLOSE	2	0	2	0	0	2	1	0	3	0	0	3	0
8	CIRCULAR RD	24	3	30	12	0	27	13	0	32	11	1	33	8
9	COTTON HILL	22	7	18	2	6	18	1	4	18	1	3	18	2
10	COTTON LANE	11	1	35	6	1	32	6	0	28	7	0	26	14
11	COTTONFIELD RD	13	3	21	0	3	21	0	3	20	1	3	19	1
12	CROMER AVENUE	18	10	12	0	8	9	0	10	10	0	10	10	0
13	DANESMOOR ROAD	39	10	23	9	10	24	8	9	23	1	9	22	4
14	DELAINE RD	29	8	19	3	9	19	3	9	18	1	8	18	1
15	ENDSLEIGH RD	3	2	7	0	0	9	0	0	9	1	0	9	0
16	EVERTT ROAD	49	8	47	4	8	48	3	8	47	3	8	40	6
17	FERNDENE RD	17	3	32	2	3	34	2	3	33	5	3	31	1
18	FERNGATE DRIVE	8	4	3	0	3	4	0	2	4	2	2	4	1
19	FRANCIS RD	16	3	14	0	2	20	2	2	16	2	1	17	4
20	FRITH RD	0	0	26	1	0	24	2	1	25	1	1	25	1
21	GORDON PLACE	11	5	3	0	2	4	0	2	4	1	2	4	3
22	GOULDEN RD	22	7	18	8	4	17	1	5	19	3	5	18	3
23	HAGUE RD	18	2	6	0	2	5	0	2	5	0	2	6	0
24	HARCOMBE RD	5	2	11	1	2	12	1	2	12	2	2	12	1
25	HEATON RD	18	9	35	1	6	37	0	4	36	3	4	35	4
26	HENSWOOD RD	57	13	14	2	11	17	2	9	18	0	8	18	1
27	HEYSCROFT ROAD	25	1	14	2	1	15	1	1	18	1	1	18	2
28	KINNAIRD RD	5	1	19	1	1	18	2	1	17	3	1	11	1
29	LYNWAY DRIVE	0	0	0	0	0	0	0	0	0	0	0	0	0
30	MARDALE AVENUE	8	1	11	0	1	10	1	1	10	2	1	9	1
31	MARRIOTT STREET	7	6	3	5	3	4	3	2	5	1	2	6	5
32	NESTON AVENUE	8	6	6	1	3	5	0	3	7	1	3	6	1
33	OAK RD	15	1	12	22	2	13	24	1	17	20	0	12	20
34	PALATINE AVENUE	13	6	9	0	5	9	0	4	9	0	4	9	0
35	PALATINE CRESCENT	10	4	0	0	3	0	1	2	0	1	2	0	1
36	PALATINE RD	6	0	3	5	1	10	13	1	7	10	0	2	0
37	PARKGATE AVENUE	7	3	10	1	3	8	0	3	8	2	2	8	2
38	PARKVILLE ROAD - KINGSLEA TO PARRS WOOD ROAD	8	3	1	0	2	10	0	3	12	0	3	10	2
39	PARKVILLE ROAD - KINGSLEA TO WILMSLOW ROAD	24	3	72	3	3	73	3	3	65	3	3	67	5
40	PRIDMOUTH RD	6	3	11	1	2	13	1	2	12	1	3	11	1
41	RATHEN RD	11	4	20	1	4	20	2	4	21	2	2	18	1
42	REDCAR AVENUE	18	9	14	1	7	11	0	7	15	0	7	14	0
43	SANDILEIGH AVENUE	42	15	28	2	15	28	6	13	27	5	10	28	5
44	TATTON GROVE	10	3	19	4	2	19	4	2	15	7	1	13	13
45	TENBY AVENUE	25	8	13	3	8	11	1	8	15	1	8	15	1
46	WENSLEY DRIVE	7	1	7	1	2	6	2	1	7	1	1	6	0
47	WILMSLOW ROAD	0	1	23	6	1	20	10	2	19	11	3	13	13
48	WOLSELEY PLACE	11	3	3	0	2	4	2	2	3	0	1	2	0
50	BRIDGELEA ROAD	12	11	8	1	9	12	1	7	11	1	5	11	0
51	LONGTON AVENUE	34	23	0	0	19	0	0	18	0	0	19	0	1
52	HOLLY ROYDE CLOSE	39	18	1	0	14	1	0	14	1	4	15	0	5
53	CANDLEFORD ROAD	1	1	8	0	1	6	0	1	5	0	1	7	1

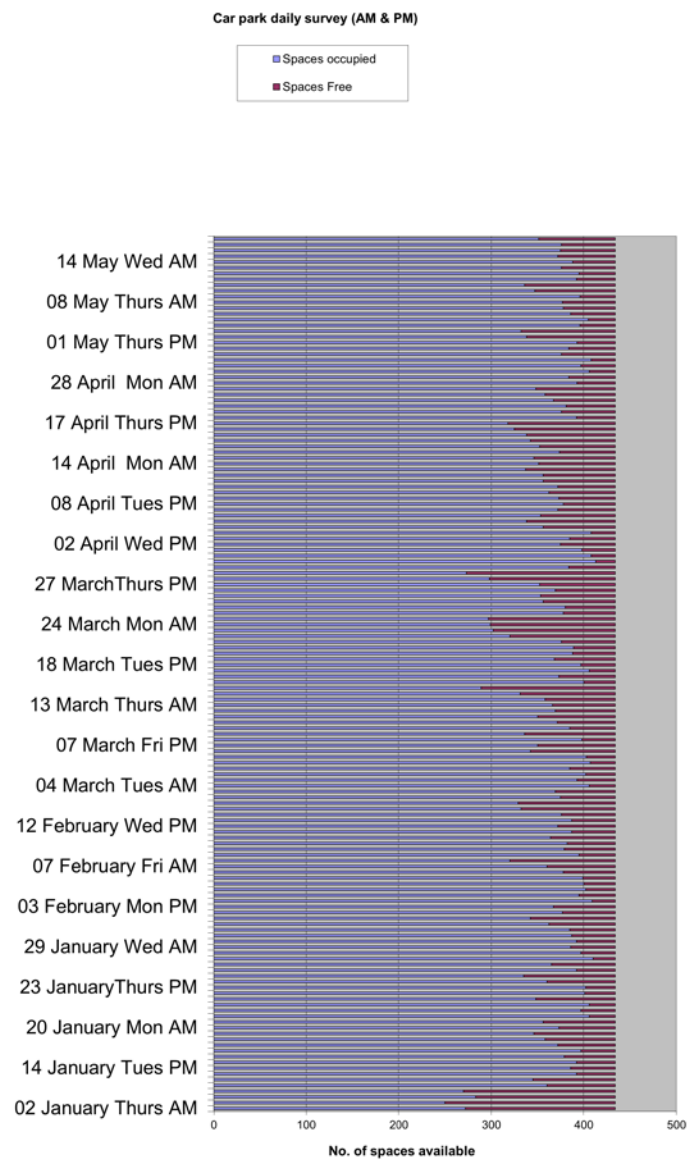
268 785 119 213 807 129 199 805 126 188 761 141

Code	Street	Overnight 01:00 - 05:00	Thursday 29/11/2012											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN RD	25	7	12	1	5	12	0	6	15	0	7	14	1
2	ALDBOROUGH CLOSE	0	1	10	0	0	11	0	0	11	0	0	8	2
3	ALVERSTONE RD	3	2	8	0	2	8	0	2	8	0	1	7	0
4	ARNFIELD RD	46	13	49	4	12	50	3	10	52	6	11	49	11
5	BURTON AVENUE	10	3	8	0	4	8	0	3	7	0	3	7	0
6	CENTRAL RD	21	8	3	4	7	3	10	7	3	1	8	3	2
7	CHATTERTON CLOSE	2	1	3	1	1	3	1	0	3	0	0	3	0
8	CIRCULAR RD	24	2	25	4	2	32	9	2	33	6	2	28	10
9	COTTON HILL	22	5	17	0	4	15	0	5	15	1	3	13	1
10	COTTON LANE	11	2	31	7	2	34	6	2	26	10	2	22	9
11	COTTONFIELD RD	13	2	22	0	2	22	0	2	23	0	1	22	0
12	CROMER AVENUE	18	7	13	0	6	14	0	7	13	0	7	13	0
13	DANESMOOR ROAD	39	13	19	7	11	20	8	12	21	12	12	18	7
14	DELAINE RD	29	9	19	1	8	21	1	6	20	0	6	18	2
15	ENDSLEIGH RD	3	0	9	0	0	10	0	0	10	1	0	10	0
16	EVERTT ROAD	49	14	45	6	14	44	5	14	44	5	14	43	3
17	FERNDENE RD	17	3	31	1	3	35	4	2	33	1	3	31	2
18	FERNGATE DRIVE	8	4	3	0	3	4	0	2	6	0	2	6	0
19	FRANCIS RD	16	3	12	3	3	16	1	2	15	1	2	17	2
20	FRITH RD	0	0	28	0	0	28	0	1	25	1	1	23	1
21	GORDON PLACE	11	6	1	2	5	2	1	5	2	0	5	2	1
22	GOULDEN RD	22	8	15	2	8	22	2	8	20	5	8	18	9
23	HAGUE RD	18	3	9	2	3	12	1	3	11	0	2	12	0
24	HARCOMBE RD	5	2	13	0	1	13	0	1	13	1	1	11	1
25	HEATON RD	18	13	26	4	12	28	4	12	28	1	12	28	4
26	HENSWOOD RD	57	11	10	1	12	11	0	12	14	2	8	12	4
27	HEYSCROFT ROAD	25	1	11	1	1	12	0	1	12	0	0	14	1
28	KINNAIRD RD	5	2	20	1	1	20	1	1	19	2	1	14	3
29	LYNWAY DRIVE	0	0	0	0	0	0	0	0	0	0	0	0	0
30	MARDALE AVENUE	8	0	11	3	0	11	3	0	10	1	0	13	1
31	MARRIOTT STREET	7	4	4	1	4	4	1	4	3	1	4	3	1
32	NESTON AVENUE	8	5	12	1	5	12	1	3	12	0	3	12	0
33	OAK RD	15	0	15	16	0	10	16	0	12	18	0	12	21
34	PALATINE AVENUE	13	5	8	1	4	8	2	5	9	2	6	7	2
35	PALATINE CRESCENT	10	5	0	0	3	1	0	3	1	0	5	0	0
36	PALATINE RD	6	0	2	4	0	14	3	0	10	5	0	9	9
37	PARKGATE AVENUE	7	2	11	0	2	11	0	3	10	1	2	8	1
38	PARKVILLE ROAD - KINGSLEA TO PARRS WOOD ROAD	8	3	4	1	3	5	0	3	7	0	3	7	1
39	PARKVILLE ROAD - KINGSLEA TO WILMSLOW ROAD	24	7	63	1	7	64	0	7	64	1	7	61	2
40	PRIDMOUTH RD	6	2	13	0	1	15	0	2	15	0	2	11	0
41	RATHEN RD	11	2	20	1	2	20	3	1	22	2	2	18	6
42	REDCAR AVENUE	18	9	13	0	8	15	2	8	13	0	8	13	0
43	SANDILEIGH AVENUE	42	14	29	3	11	26	3	13	29	3	13	24	3
44	TATTON GROVE	10	0	17	4	0	18	6	1	16	6	1	16	9
45	TENBY AVENUE	25	9	15	0	8	15	0	9	15	0	9	15	0
46	WENSLEY DRIVE	7	3	5	0	2	5	1	2	6	1	2	4	1
47	WILMSLOW ROAD	0	1	16	12	1	20	13	1	16	12	1	1	8
48	WOLSELEY PLACE	11	4	3	0	4	4	0	4	4	0	4	4	0
50	BRIDGELEA ROAD	12	9	10	0	9	11	1	6	12	1	5	11	0
51	LONGTON AVENUE	34	22	0	1	20	0	0	18	0	0	20	0	2
52	HOLLY ROYDE CLOSE	39	21	0	1	12	0	2	15	0	3	16	0	3
53	CANDLEFORD ROAD	1	1	7	0	1	8	0	1	6	0	1	7	0

273 750 102 239 807 114 237 794 113 236 722 146







Code	Street	01:00 - 05:00	11/11/2013											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stay	Residents	Visitor - Long Stay	Visitor - Short Stay	Residents	Visitor - Long Stay	Visitor - Short Stay	Residents	Visitor - Long Stay	Visitor - Short Stay
1	ALAN ROAD	19	8	20	3	10	22	2	7	24	4	7	23	2
2	ALDBOROUGH CLOSE	2	0	11	0	0	9	0	0	10	0	0	9	0
3	ALVERSTONE ROAD	4	2	11	1	2	11	0	2	7	0	0	3	0
4	ARNFIELD ROAD	49	34	54	5	22	67	6	19	61	0	15	56	3
5	BRIDGELEA ROAD	9	7	13	1	7	13	2	7	12	1	5	13	3
6	BURTON AVENUE	10	5	1	0	6	2	0	5	1	0	4	1	0
7	CANDLEFORD ROAD	1	1	7	0	1	7	0	1	7	1	1	4	0
8	CENTRAL ROAD	18	8	5	1	6	2	0	6	2	0	8	3	1
9	CHATTERTON CLOSE	2	1	2	1	1	2	1	0	2	0	1	2	0
10	CIRCULAR ROAD	22	4	29	18	4	29	0	1	13	0	1	12	2
11	COTTON HILL	21	6	18	3	7	18	4	8	16	3	7	11	0
12	COTTON LANE	18	6	27	2	1	30	3	6	30	2	6	26	3
13	COTTONFIELD ROAD	11	3	21	2	2	21	2	4	17	0	3	11	0
14	CROMER AVENUE	16	9	13	1	8	14	1	6	14	0	7	11	1
15	DANESMOOR ROAD	28	11	22	5	9	29	9	10	27	12	8	24	6
16	DELAINE ROAD	30	27	19	4	20	21	5	12	19	5	11	16	1
17	ENDSLEIGH ROAD	8	8	2	1	7	2	1	6	1	1	5	1	1
18	EVERTT ROAD	33	11	25	4	10	32	5	7	36	2	7	26	3
19	FERNDENE ROAD	25	9	39	4	9	39	4	7	35	2	5	24	1
20	FERNGATE DRIVE	7	6	3	0	6	3	0	6	2	0	5	1	0
21	FRANCIS ROAD	10	6	12	3	5	10	0	5	12	1	5	12	1
22	FRITH ROAD	0	0	0	0	0	0	0	0	0	0	0	0	0
23	GORDON PLACE	8	5	3	1	7	4	0	7	4	0	6	4	0
24	GOULDEN ROAD	28	9	28	1	8	26	0	8	27	2	8	24	2
25	HAGUE ROAD	30	12	4	1	7	5	1	2	1	1	2	1	2
26	HARCOMBE ROAD	3	2	10	2	1	15	2	1	16	0	1	13	1
27	HEATON ROAD	13	9	21	2	6	21	2	10	22	3	9	21	2
28	HENSWOOD ROAD	67	19	9	0	17	10	6	17	10	2	18	8	5
29	HEYSCROFT ROAD	9	3	8	1	3	9	3	3	8	2	3	8	2
30	HOLLY ROYDE CLOSE	30	14	3	0	12	2	0	16	2	0	14	2	1
31	KINNAIRD ROAD	1	0	2	0	0	2	0	0	2	0	0	2	0
32	LONGTON AVENUE	30	16	0	1	12	1	0	11	1	1	11	1	3
33	LYNWAY DRIVE	1	0	0	0	0	1	0	0	0	0	0	0	0
34	MARDALE AVENUE	12	6	5	0	6	5	0	6	5	0	6	5	0
35	MARRIOTT STREET	10	3	10	3	3	14	6	3	14	4	3	13	3
36	NESTON AVENUE	13	3	3	0	3	4	0	3	3	0	3	2	0
37	OAK ROAD	20	2	14	14	1	22	12	1	21	9	1	21	7
38	PALATINE AVENUE	12	4	2	1	3	3	1	3	4	0	4	3	0
39	PALATINE CRESCENT	9	2	2	1	3	3	0	3	3	0	3	2	0
40	PALATINE ROAD	7	4	15	7	3	33	13	4	27	15	3	13	6
41	PARK GATE AVENUE	9	4	0	0	3	2	0	2	2	0	2	2	0
42	PARKVILLE ROAD - KINGSLEA TO WILMSLOW	24	6	88	3	6	87	2	6	87	1	5	81	2
43	PARKVILLE ROAD - KINGSLEAD TO PARRS WOOD	10	1	16	0	1	16	0	2	16	2	1	13	2
44	PRIDMOUTH ROAD	9	5	13	2	3	13	3	2	12	1	2	12	1
45	RATHEN ROAD	14	5	17	3	2	16	0	2	16	0	2	15	0
46	REDCAR AVENUE	18	6	5	0	6	5	2	6	6	1	6	6	0
47	SANDILEIGH AVENUE	38	25	17	4	14	18	5	13	18	3	11	16	2
48	TATTON GROVE	20	6	13	10	4	12	10	4	13	5	4	12	6
49	TENBY AVENUE	24	13	10	1	11	10	0	11	9	1	9	9	2
50	WENSLEY DRIVE	7	4	5	1	4	5	0	5	6	0	5	4	1
51	WILMSLOW ROAD	0	0	11	11	0	17	37	0	17	25	0	17	20
52	WOLESLEY PLACE	13	7	5	0	7	5	1	8	5	1	8	5	0

Code	Street	01:00 - 05:00	12/11/2013											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN ROAD	19	10	31	5	11	37	10	9	32	3	6	21	2
2	ALDBOROUGH CLOSE	2	0	11	0	0	11	0	0	11	0	0	10	0
3	ALVERSTONE ROAD	4	2	8	0	2	7	0	2	2	0	2	4	0
4	ARNFIELD ROAD	49	20	57	9	19	63	5	15	57	5	14	45	1
5	BRIDGELEA ROAD	9	4	25	4	4	23	3	4	12	0	3	8	0
6	BURTON AVENUE	10	5	2	1	4	2	1	4	2	1	4	1	0
7	CANDLEFORD ROAD	1	0	6	0	0	6	0	0	6	0	0	6	1
8	CENTRAL ROAD	18	6	3	3	5	4	1	5	3	0	5	3	0
9	CHATTERTON CLOSE	2	2	1	0	2	1	0	1	1	0	1	1	0
10	CIRCULAR ROAD	22	6	28	22	5	26	20	5	17	1	2	6	4
11	COTTON HILL	21	6	21	2	4	21	2	5	22	1	5	22	1
12	COTTON LANE	18	10	38	4	10	40	2	6	42	4	5	42	4
13	COTTONFIELD ROAD	11	1	22	1	1	22	1	1	23	0	1	22	0
14	CROMER AVENUE	16	10	12	0	10	12	0	10	12	0	9	9	0
15	DANESMOOR ROAD	28	8	25	3	9	25	1	8	25	3	6	22	2
16	DELAINE ROAD	30	10	29	1	10	27	0	10	14	2	10	14	4
17	ENDSLEIGH ROAD	8	5	3	0	6	3	0	4	1	0	4	1	2
18	EVERITT ROAD	33	11	27	3	7	32	3	7	32	2	8	29	4
19	FERNDENE ROAD	25	7	39	2	7	37	2	8	37	3	7	32	4
20	FERNGATE DRIVE	7	2	5	2	2	5	1	2	5	1	2	5	0
21	FRANCIS ROAD	10	5	15	1	5	16	1	5	18	0	5	19	0
22	FRITH ROAD	0	0	0	0	0	0	0	0	0	0	0	0	0
23	GORDON PLACE	8	4	3	0	5	5	0	6	3	1	4	2	0
24	GOULDEN ROAD	28	6	22	7	6	22	2	7	15	1	5	16	2
25	HAGUE ROAD	30	5	7	4	7	7	3	8	4	0	1	2	1
26	HARCOMBE ROAD	3	0	12	1	0	13	1	0	6	1	0	6	1
27	HEATON ROAD	13	7	24	2	7	21	4	10	21	1	7	22	1
28	HENSWOOD ROAD	67	16	18	3	17	16	1	17	14	1	16	12	1
29	HEYSCROFT ROAD	9	2	21	1	2	21	1	3	21	0	3	22	0
30	HOLLY ROYDE CLOSE	30	12	2	0	13	3	0	12	3	0	13	0	0
31	KINNAIRD ROAD	1	0	0	0	0	0	0	0	0	0	0	0	0
32	LONGTON AVENUE	30	16	3	4	18	2	2	13	1	1	12	1	4
33	LYNWAY DRIVE	1	0	0	0	0	0	0	0	0	0	0	0	1
34	MARDALE AVENUE	12	4	8	1	4	8	0	2	10	0	2	10	0
35	MARRIOTT STREET	10	3	14	0	3	8	0	2	10	1	2	11	1
36	NESTON AVENUE	13	4	5	0	4	5	0	3	5	0	3	4	1
37	OAK ROAD	20	0	14	13	0	9	13	0	11	19	0	11	13
38	PALATINE AVENUE	12	7	4	0	7	4	0	5	4	0	3	4	0
39	PALATINE CRESCENT	9	3	1	0	3	2	0	3	0	0	3	2	1
40	PALATINE ROAD	7	3	30	8	3	37	21	4	28	11	3	8	1
41	PARK GATE AVENUE	9	3	2	1	2	3	1	2	3	1	2	2	0
42	PARKVILLE ROAD - KINGSLEA TO WILMSLOW	24	6	76	2	5	77	2	5	79	2	4	74	4
43	PARKVILLE ROAD - KINGSLEAD TO PARRS WOOD	10	2	22	2	0	27	0	1	27	0	1	27	2
44	PRIDMOUTH ROAD	9	3	17	1	2	18	1	2	13	1	2	13	2
45	RATHEN ROAD	14	3	22	0	3	22	0	3	22	2	3	22	2
46	REDCAR AVENUE	18	5	6	0	2	5	1	4	7	2	6	6	2
47	SANDILEIGH AVENUE	38	16	17	4	11	17	1	10	16	1	9	13	2
48	TATTON GROVE	20	3	16	8	1	13	4	2	14	6	1	11	5
49	TENBY AVENUE	24	12	11	0	8	9	1	9	10	2	7	11	1
50	WENSLEY DRIVE	7	5	4	1	4	4	2	4	4	1	5	5	0
51	WILMSLOW ROAD	0	1	22	16	2	32	10	2	31	18	1	22	10
52	WOLESLEY PLACE	13	6	3	0	4	4	3	5	1	1	4	2	1

Code	Street	01:00 - 05:00	13/11/2013											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN ROAD	19	9	34	2	8	33	4	8	35	3	12	29	2
2	ALDBOROUGH CLOSE	2	0	8	0	1	6	0	0	8	0	2	4	1
3	ALVERSTONE ROAD	4	3	5	1	3	5	1	3	5	1	3	2	0
4	ARNFIELD ROAD	49	17	58	7	18	63	8	19	58	5	18	52	2
5	BRIDGELEA ROAD	9	5	23	0	5	26	0	5	19	0	4	15	1
6	BURTON AVENUE	10	3	3	1	5	2	0	5	2	0	5	4	0
7	CANDLEFORD ROAD	1	0	7	0	0	8	0	0	7	0	0	5	0
8	CENTRAL ROAD	18	5	6	1	6	6	0	6	2	0	6	3	0
9	CHATTERTON CLOSE	2	1	3	0	1	3	0	1	3	0	0	1	0
10	CIRCULAR ROAD	22	3	30	9	4	29	7	4	31	7	2	12	8
11	COTTON HILL	21	8	19	0	5	23	1	7	21	1	6	21	3
12	COTTON LANE	18	4	36	1	6	42	5	2	36	2	3	24	4
13	COTTONFIELD ROAD	11	5	20	1	5	22	0	5	22	0	4	12	0
14	CROMER AVENUE	16	9	13	0	8	13	0	7	13	0	7	13	1
15	DANESMOOR ROAD	28	9	22	3	8	25	3	9	24	1	8	22	1
16	DELAINE ROAD	30	10	26	2	10	25	1	8	27	1	11	21	1
17	ENDSLEIGH ROAD	8	3	7	0	2	7	0	2	5	0	1	2	0
18	EVERTT ROAD	33	15	28	2	11	29	2	11	30	3	8	22	5
19	FERNDENE ROAD	25	9	33	3	8	36	1	8	37	0	7	32	0
20	FERNGATE DRIVE	7	3	4	1	1	5	0	2	4	0	2	4	2
21	FRANCIS ROAD	10	5	7	2	6	17	3	6	9	2	4	14	1
22	FRITH ROAD	0	0	0	0	0	0	0	0	0	0	0	0	0
23	GORDON PLACE	8	5	5	1	4	8	1	5	5	0	5	4	0
24	GOULDEN ROAD	28	5	26	4	5	26	2	4	24	2	4	22	2
25	HAGUE ROAD	30	5	9	3	5	8	2	5	5	0	1	0	2
26	HARCOMBE ROAD	3	0	15	1	0	18	1	0	13	1	0	6	2
27	HEATON ROAD	13	12	19	3	10	21	2	10	18	1	8	16	5
28	HENSWOOD ROAD	67	18	9	1	15	14	1	15	14	1	16	13	2
29	HEYSCROFT ROAD	9	4	18	0	4	18	4	5	15	4	3	12	1
30	HOLLY ROYDE CLOSE	30	11	1	0	13	1	1	19	1	1	15	1	0
31	KINNAIRD ROAD	1	0	0	0	0	0	0	0	0	0	0	0	0
32	LONGTON AVENUE	30	12	6	2	11	7	3	9	7	2	7	6	3
33	LYNWAY DRIVE	1	0	0	0	0	0	0	0	0	0	0	0	0
34	MARDALE AVENUE	12	4	6	1	4	8	0	4	8	0	4	7	0
35	MARRIOTT STREET	10	3	10	1	2	11	1	2	14	0	2	10	0
36	NESTON AVENUE	13	6	5	1	5	5	0	4	4	0	3	2	0
37	OAK ROAD	20	3	10	11	1	8	23	0	11	18	0	6	5
38	PALATINE AVENUE	12	3	5	2	3	6	0	2	6	0	2	6	0
39	PALATINE CRESCENT	9	4	1	0	2	2	1	3	1	0	3	1	0
40	PALATINE ROAD	7	4	33	6	4	43	14	4	47	3	2	31	4
41	PARK GATE AVENUE	9	5	4	2	3	3	0	3	3	0	3	1	0
42	PARKVILLE ROAD - KINGSLEA TO WILMSLOW	24	5	84	1	5	66	0	5	69	1	4	68	2
43	PARKVILLE ROAD - KINGSLEAD TO PARRS WOOD	10	2	16	4	2	15	3	1	15	5	1	17	4
44	PRIDMOUTH ROAD	9	4	18	0	2	20	0	3	19	2	3	12	2
45	RATHEN ROAD	14	3	21	4	4	23	2	4	18	2	3	14	4
46	REDCAR AVENUE	18	7	7	0	9	10	0	8	9	3	7	8	2
47	SANDILEIGH AVENUE	38	18	19	6	16	20	2	20	19	3	14	16	5
48	TATTON GROVE	20	5	15	9	3	17	6	3	13	9	3	8	6
49	TENBY AVENUE	24	12	14	0	12	14	0	11	14	0	10	10	0
50	WENSLEY DRIVE	7	4	5	0	3	6	0	3	6	1	2	6	1
51	WILMSLOW ROAD	0	0	9	10	2	23	26	2	24	12	0	18	14
52	WOLESLEY PLACE	13	8	3	2	7	3	1	5	2	1	0	0	0

Code	Street	01:00 - 05:00	14/11/2013											
			09:00			11:00			13:00			15:00		
			Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays	Residents	Visitor - Long Stay	Visitor - Short Stays
1	ALAN ROAD	19	8	22	2	6	25	5	5	30	5	7	23	3
2	ALDBOROUGH CLOSE	2	2	9	0	2	9	0	2	9	0	1	5	0
3	ALVERSTONE ROAD	4	2	8	0	1	9	0	2	9	0	2	5	0
4	ARNFIELD ROAD	49	17	57	2	17	64	5	16	63	5	16	61	2
5	BRIDGELEA ROAD	9	9	17	1	8	13	2	7	12	2	4	11	1
6	BURTON AVENUE	10	6	3	1	6	3	1	6	3	0	6	3	0
7	CANDLEFORD ROAD	1	0	8	0	0	8	0	0	8	0	0	6	1
8	CENTRAL ROAD	18	5	4	0	7	7	5	7	7	3	8	6	1
9	CHATTERTON CLOSE	2	1	3	0	1	2	0	1	2	2	1	2	1
10	CIRCULAR ROAD	22	4	25	8	3	28	12	3	29	9	1	10	8
11	COTTON HILL	21	6	18	0	4	21	0	3	17	0	3	17	0
12	COTTON LANE	18	6	42	3	3	36	7	4	37	6	2	36	12
13	COTTONFIELD ROAD	11	5	20	0	4	15	2	5	15	6	5	14	3
14	CROMER AVENUE	16	8	13	1	7	13	1	6	11	1	8	8	1
15	DANESMOOR ROAD	28	6	25	3	6	25	3	10	23	7	8	21	5
16	DELAINE ROAD	30	14	28	2	8	28	1	8	26	2	10	23	0
17	ENDSLEIGH ROAD	8	3	8	0	3	7	0	3	7	0	2	4	0
18	EVERTT ROAD	33	11	24	3	8	31	4	8	33	2	9	31	2
19	FERNDENE ROAD	25	9	29	4	9	29	2	8	30	2	9	26	1
20	FERNGATE DRIVE	7	2	5	0	2	5	0	2	4	0	2	4	0
21	FRANCIS ROAD	10	4	13	1	7	11	2	7	20	1	7	17	3
22	FRITH ROAD	0	0	0	0	0	0	0	0	0	0	0	0	0
23	GORDON PLACE	8	5	2	3	8	3	0	5	4	1	4	4	1
24	GOULDEN ROAD	28	7	18	2	6	23	3	5	25	5	4	22	4
25	HAGUE ROAD	30	4	6	0	5	5	1	5	7	0	1	2	1
26	HARCOMBE ROAD	3	2	13	4	1	15	0	1	14	0	1	6	1
27	HEATON ROAD	13	9	20	2	11	20	1	9	21	3	9	18	2
28	HENSWOOD ROAD	67	18	10	5	0	0	0	0	0	0	0	0	0
29	HEYSCROFT ROAD	9	2	21	0	2	22	1	2	25	0	2	21	0
30	HOLLY ROYDE CLOSE	30	11	1	2	12	2	1	11	2	1	12	2	0
31	KINNAIRD ROAD	1	0	0	0	0	0	0	0	0	0	0	0	0
32	LONGTON AVENUE	30	10	5	2	13	3	1	13	5	0	10	4	2
33	LYNWAY DRIVE	1	0	0	0	0	0	0	0	0	0	0	0	1
34	MARDALE AVENUE	12	6	7	0	4	7	0	5	7	3	5	7	2
35	MARRIOTT STREET	10	2	10	1	2	11	2	3	10	3	3	8	2
36	NESTON AVENUE	13	3	3	1	2	2	0	1	2	2	2	1	3
37	OAK ROAD	20	0	9	23	3	12	13	0	16	16	0	13	14
38	PALATINE AVENUE	12	3	4	4	1	4	3	3	2	1	2	2	1
39	PALATINE CRESCENT	9	2	1	0	2	1	3	2	0	1	3	0	1
40	PALATINE ROAD	7	0	23	1	3	35	4	2	39	9	1	36	8
41	PARK GATE AVENUE	9	4	1	0	2	2	0	1	2	0	4	0	0
42	PARKVILLE ROAD - KINGSLEA TO WILMSLOW	24	5	81	4	3	81	3	4	84	2	3	81	1
43	PARKVILLE ROAD - KINGSLEAD TO PARRS WOOD	10	0	17	3	0	18	7	0	19	5	2	13	10
44	PRIDMOUTH ROAD	9	4	16	0	3	16	1	2	16	1	4	9	0
45	RATHEN ROAD	14	4	22	1	3	16	0	3	11	0	0	5	6
46	REDCAR AVENUE	18	9	4	0	9	6	2	11	6	0	10	6	0
47	SANDILEIGH AVENUE	38	18	19	6	19	25	6	16	25	3	11	23	3
48	TATTON GROVE	20	3	11	0	5	13	4	3	17	4	3	14	5
49	TENBY AVENUE	24	13	11	0	13	10	0	13	9	0	14	7	0
50	WENSLEY DRIVE	7	3	4	1	4	5	0	3	7	1	3	7	0
51	WILMSLOW ROAD	0	0	7	10	3	27	20	2	25	11	1	19	15
52	WOLESLEY PLACE	13	4	6	0	5	5	1	5	6	1	0	0	0







Green Travel Plan

1. Executive summary

This Green Travel Plan has been produced to support the Strategic Planning Framework (SPF) for The Christie main Withington site. It will be reviewed and updated in August 2014 and annually thereafter. The Trust as an organisation committed to sustainable development has developed this strategy with the aim to reduce the number of single occupancy vehicles travelling to the site.

Current data that will form the baseline from which this strategy will be measured against indicates 34.7% of staff commute via sustainable travel.

Key support mechanisms currently in place include:

- **Development & engagement:** Travel plan co-ordinator, Sustainable travel policy, travel & car parking group and neighbourhood forum.
- **Monitoring:** Staff survey and postcode analysis.
- **Communication:** Intranet page focusing on sustainable travel and promotional events (e.g. annual health & wellbeing day).
- **Walking:** Route planning
- **Cycling:** Bike to work scheme and route planning.
- **Public transport:** Discounted tickets and interest free loans for season tickets.
- **Car share:** Discounted permits to car sharers and priority parking.

The new modal shift target is to aim high and ultimately reach a desired 60% of staff using sustainable travel by 2030.

Mode	Modal split 2013 (%) - Baseline	Modal split 2019 target (%) [Short term]	Modal split 2024 target (%) [Medium term]	Modal split 2030 target (%) [Long term]
Sustainable travel	34.7	48	52	60

Key actions to achieve the modal shift are listed below

- Agree sustainable fund
- Monitoring procedure
- Indicators
- Means testing (car parking)
- Review parking fees
- Adaptable targets
- Marketing (Potential employees)
- Marketing (New employees)
- Marketing (Current employees)
- Public transport concessions

2. Introduction and purpose

This Green Travel Plan has been prepared in partnership between The Christie NHS Foundation Trust (the Trust) and Manchester City Council (MCC).

The Christie is a dynamic place that constantly needs to respond to the latest advances in research and treatment, often with new development. Within a surrounding residential context, future developments need to be appropriately planned in order to mitigate potential transport and parking impacts.

The Christie is a major generator of travel in south Manchester. There are two distinct groups of travellers to the site, employees and patients (and their visitors). There are some 240,000 patient visits to the site each year. On the busiest days there are just under 1,000 patients treated at the site. There are about 2,700 people employed on the site, including just over 300 employed by the University of Manchester in the Paterson Building, with the Trust employing approximately 2,400 staff. Due to the predominance of outpatient care the majority at the Christie work the “day shift” albeit with many different hours worked.

Initial estimates show that developments within the lifespan of the SPF will create over 550 new jobs. The Trust do not expect there to be a significant increase in the numbers of patients visiting the site. The SPF indicates that of the new proposed developments it is only the Proton Building that will result in additional patients being treated at the site. It is anticipated that this will generate the requirement for 16 additional patient parking spaces.

The purpose of this document is therefore to provide an agreed transport strategy for the ongoing development of the Withington site. The strategy will aim to reduce the number of single occupancy vehicles travelling to the site. This document will act as the foundation for a comprehensive sustainable travel communication strategy and is intended to cover up to the year 2030.

3. Baseline

The Trust has successfully achieved modal shift targets from a previous five year plan and is committed to sustainable development. The intention is to support the Manchester City Council carbon reduction schemes and address the Trust’s commitment, to good corporate citizenship. Data to monitor progress on the modal shift is obtained annually through the formal Trust staff survey.

To provide consistency and assurance, the new survey is undertaken by an external organisation in November, with a formal results report. At this moment in time the survey only goes out to those staff on the Christie payroll, not other site users. The current survey indicates 34.7% of staff commute via sustainable travel. These results will form the baseline against which this strategy will be measured.

3.1 Baseline support mechanisms

The following table details the support mechanism currently in place that will form a baseline for the trust, giving an understanding of where we are and where the improvements could be made to achieve the targets set out within this strategy.

Area	Mechanism currently in place
Development & engagement	Travel plan co-ordinator in place
	Sustainable travel policy
	Travel choices employers forum membership
	Partnership with Transport for Greater Manchester (TfGM)
	Meetings with Manchester city council
	Neighbourhood Forum
	Sustainable Development Committee
Monitoring	Travel & car parking group
	Annual staff survey takes place to collate modal shift figures.
	Post code analysis conducted annually
	Modal shift report annually submitted to MCC under L/BH/EVD3001/2984 106 agreement.
Communication & marketing	The Christie travel page activity monitored
	The Christie travel page (Focusing on sustainable travel)
	Internal publications (staff magazine, team brief and posters)
	Internal signage for bus stops and stop information points at all main entrances
	Events (e.g. health & wellbeing day, national cycle to work day and ad-hoc promotions)
	Patient ‘getting to the Christie’ leaflets focus on sustainable travel
Walking initiative	Bus timetables now centrally located on the glass corridor
	Online route planning available
Cycling initiative	Bike to work scheme available (Contract with Cyclescheme until June 2016)
	124 cycle spaces available
	Bike user group formed (BUG) - Online
	3 Showers available to all staff
Public transport initiative	Online route planning available
	Interest free public transport loans
	Staff discounts through Stagecoach Manchester (weekly, monthly and annually) and Metrolink annual season tickets.
	Stagecoach agreed to add The Christie to scrolling destination information.
Car share initiative	Stagecoach added The Christie to scrolling destination information.
	Car share scheme – reduced fee, accelerated application and dedicated spaces (29)

4. Modal shift targets

The new modal shift target is to aim high with the following targets for staff using sustainable travel:

- Short term (2019) - 48%
- Medium term (2024) – 52%
- Long term (2030) – 60%

The table below provides a step by step break down of the modal shift and where the Trust aims to be each year.

External initiative	Internal initiative	YEAR	TARGET MODAL SHIFT	SURVEY
	New baseline	Year 2013	35	35
<ul style="list-style-type: none"> • S106 Parking restrictions • 'Get me there' introduced on metrolink • The Busway (LSM) • First Manchester - Cross city bus route 	<ul style="list-style-type: none"> • Metrolink staff discount introduced (10%) • Communication material for potential employees • AECOM event (promotion to current employees) • Twice yearly bike doctor • Car share database cleanse and re-launch • Pedestrian access assessments • Additional car sharer spaces provided • Promotion (Potential employees) 	Year 2014	36	
<ul style="list-style-type: none"> • Cross city bus (bus priority lanes) • 'Get me there' introduced on bus services 	<ul style="list-style-type: none"> • Introduction of means testing • Promotional pack for new employees • Improved internal & external public transport signage • Improvements to cycling facilities • On site cycle training days • Increased motorcycle parking • Review parking fees • Provide support for occasional car users • Investigate salary sacrifice options for public transport tickets • Review creating a guaranteed ride home for car sharers. • 'Walking buddy groups' 	Year 2015	40	
	• Means test current permit holders	Year 2016	44	
• 'Get me there' introduced on train services	• Promote completion of 'Get me there' and negotiate setting up a corporate scheme.	Year 2017	45	
		Year 2018	45	
		Year 2019	48	
		Year 2020	48	
		Year 2021	48	
		Year 2022	48	
		Year 2023	48	
		Year 2024	52	
• Vélo city 2025		Year 2025	53	
		Year 2026	54	
		Year 2027	55	
		Year 2028	55	
		Year 2029	58	
		Year 2030	60	

5. Transport strategy action plan

Actions are listed in order of priority					
Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
01	Finance	Agree sustainable fund	Create a fund, with growth from baseline assessment, for all business cases to consider sustainable travel requirements Review possibility of ring fencing finances to provide additional support for funding green travel initiatives	Provide the financial support for travel strategy aim	TBA
02	Monitoring	Monitoring procedure	Agree method of monitoring modal shift progress in transport strategy and risk elevation process should targets not be being achieved.	Allow for assessment of the effectiveness overall strategy.	Following SPF approval
03	Monitoring	Indicators	Indicators are required to monitor the effectiveness of the transport strategy actions. The current suggestions include monitoring public transport tickets sold internally, interest free public transport loans issued, cycle spaces occupied, car sharers and hits on the travel plan website	Allow for assessment of the effectiveness individual actions taken and the process being made.	Following SPF approval
04	Car Parking	Means testing	Means test parking requirements based on the following criteria: <ul style="list-style-type: none">• Live in excess of X miles (to be agreed following postcode analysis of permit holders) and do not have a public transport connection of more than 40 minutes and no more than a maximum of 1 change.• Blue badge holders or long term health issues.• Registered in the Trust car sharer scheme.• Require car for patient related services• Multi-site staff• Start & finish times• Permanent nights Car parking procedure will be updated to apply this action.	Support the move to sustainable modes of transport as car parking will only be available for those with no practicable alternative. Initial implication to any new staff and also communication in job adverts.	Dec-15

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
05	Car Parking	Means test on current permit holders	See Action 04	See Action 04	Dec-16
06	Car Parking	Review parking fees	Review parking fees as a possible control to discourage single occupancy vehicles	Reduce the number of single occupancy vehicles	April-15
07	Monitoring	Adaptable targets	Adapt targets as and when major elements of Manchester's green travel network go live (e.g. cross city bus, 'Get me there' and Vélocity).	Reduction in single occupancy car travel due to green travel network improvements	Follow release of an element
08	Procedural	Provide support for occasional car users	Review potentially giving support to staff that could use sustainable modes if they were allowed to use there car on certain days.	Currently occasional users or potential occasional users must be full time permit holders - no middle ground is available.	Dec-15
09	Monitoring	Annually conduct a post code analysis	Conduct an annual post code analysis of all staff employed on Christie Wilmslow Road site. This will include all site users - e.g. The Christie Clinic, CRUK-MI and WMIC.	Allow for transport strategy to be adapted in line with current staff distribution	Ongoing

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
10	Communication & marketing	Promotion (Potential employees)	Promote the Christie as a sustainable organisation with sustainable travel prioritised over single occupancy vehicle (SOV) Formal meetings with the communications team to develop promotion.	Employees who are considering working at the Christie will have been made aware that SOV is to be discouraged and that a means test will be required for permit application.	Sep-14
11	Communication & marketing	Promotion (New employees)	Introduce the Christie as a sustainable organisation with alternatives to SOV in place and make it clear that permit applicants must have met with travel planning representatives. Formal meetings will take place with communications to develop promotion.	New employees will be supplied with an information pack detailing alternatives to SOV and that a means test will be required for permit application.	Dec-15
12	Communication & marketing	Promotion (Current employees)	Promoting alternatives to SOV via intranet (interactive travel plan), posters, events, notice boards and personal travel planning*. Formal meetings with the communications team to develop promotion. *AECOM event -Staff will be encouraged to attend.	Information will be widely available on the alternatives to SOV.	Ongoing

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
13	Public transport initiative	Continued negotiations for public transport concessions	Maintain communication with service providers to negotiate concessions	Reduce the cost of public transport therefore increase its appeal to staff.	Ongoing
14	Public transport initiative	Investigate salary sacrifice for public transport tickets	Review potential for salary sacrifice to pay for public transport tickets (subject to HRMC approval)	Reduce the cost of public transport therefore increase its appeal to staff.	April-15
15	Public transport initiative	Review internal funded public transport incentives * * Potential Finance (HMRC) & Human resource implications	Investigate potential internal funding to add additional reductions to public transport costs.	Reduce the cost of public transport therefore increase its appeal to staff.	TBA
16	Public transport initiative	TfGM overall tickets 'Get me there'	Promote the launch of 'Get me there' a one card pays for all public transport modes in Greater Manchester. Negotiate the setting up of a corporate scheme with TfGM to allow the Trust to sell 'Get me there' cards.	Employees will be aware of the scheme and have on site access to the scheme.	2017 (TBA by TfGM)
17	Public transport initiative	Signage (Public transport wayfinder)	A further internal and external review of all signage & information points to direct staff, patients & visitors to public transport	Public transport links are promoted so people are aware geographically of the alternatives.	April-15

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
18	Communication & marketing	Park & Ride (TfGM)	Promotion of the Metrolink park & ride	Increased use of park & ride by Christie staff, patients and visitors	Jul-14
19	Public transport initiative	Cross city bus (Bus priority lanes)	Discuss with TfGM how the Trust can be linked in with the Cross city bus project	Improve awareness of public transport network.	2015 (TBA by TfGM)
20	Public transport initiative	Investigate pool bicycles	Trial the use of pool bicycles to connect public transport station to the Trust (i.e. West Didsbury Metrolink, Burnage railway & Mauldeth Road railway).	Improve public transport connection to the Christie	TBA- Deadline will be reviewed upon discussions with TfGM
21	Cycling initiative	Review of current resources	Complete a review of cycle provisions (i.e. parking, showers, lockers, changing rooms and clothes drying facilities)	Find potential improvements in cycling resources to encourage and support cycling.	Aug-14
22	Cycling initiative	Action any recommendations from Action 21	See Action 21 and apply recommendations via business cases	Make improvements to cycling resources to encourage and support cycling.	April-15

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
23	Cycling initiative	On site cycle training days	Provide onsite training days for all skill levels of riding and cycle maintenance	Support modal shift over to cycling.	April-15
24	Cycling initiative	Employee bike hire scheme	Allow for employees to have a go before buying their own bike	Support modal shift over to cycling.	TBA- Deadline will be reviewed upon discussions with TfGM
25	Cycling initiative	Twice yearly bike doctor	Contractor hired to conduct servicing of staff bikes.	Support staff who cycle to work.	Sep-14
26	Cycling initiative	Support Vélocity 2025	Liaise with TfGM and MCC on Vélocity 2025 a project to build a cycling culture and infrastructure	Incorporation into the infrastructure with cycle lanes linked to main site and staff reassured about road safety with improved bicycle lanes.	Ongoing
26	Car share initiative	Review internal and car share database and launch external database (Car share GM)	Conduct full review & maintenance of the car share internal database* and instigate the external car share database * Annually thereafter	Ease of use for finding a car share partner	Sep-14

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
27	Car share initiative	Guaranteed Ride Home scheme for car sharers	Review the possibility of a backup plan for car sharers should they or their car share partner have to leave site earlier or later than expected.	Support car sharing by providing a Trust backup plan.	April-15
28	Car share initiative	Increase designated car share spaces	Currently there are 29 front of house car sharer spaces. Review increasing numbers in line with modal shift requirements and as and when only 2 spaces remain.	Car share spaces filled with a minimum of 2 available to encourage more sharers.	Ongoing
29	Walking initiative	Pedestrian access assessments	Identify whether footpaths around the site are safe, secure, well lit, maintained and free from obstructions	Provide safe access for pedestrians	Jun-14
30	Walking initiative	In line with Christie commitment staff pledge on supporting a Health workplace - Implement walking buddy groups	Review possibility of creating walking groups during lunch periods to encourage walking as a healthy lifestyle choice	Staff take an interest in walking as a social activity may also start walking to work	May-15
31	Human Resources	Flexible hours	Review the possibility flexible work hours to avoid peak traffic and fit in with public transport schedules. Consider possible shift alterations to reduce the need for travel (i.e. 12 hours shifts).	Increase the appeal of public transport and reduce the number of journeys that need to be made to site.	TBA

Action N ^o	Action	Sub-action	Action in detail	Desired outcome	Timescale
32	Off Site Alternatives	Satellite offices	Review possible satellite offices with staff distribution data	Situate satellite offices in a location that involves a lower carbon footprint produced through commuting	To be agreed with capital team as and when developments occur
33	Off Site Alternatives	Relocation	Review possible relocate back of house staff off the main site to satellite offices	Reduce the traffic volume coming to the main site.	To be agreed with capital team as and when developments occur
34	Off Site Alternatives	Park & Ride	Implementation of a park and ride scheme noting Manchester City Councils recommendation for setting them up near a Metrolink station	Reduces number of vehicles travelling to main site	To be agreed with capital team as and when developments occur
35	Car Parking	Electrical cars	Provide an onsite recharge point for electrical vehicles	Improved local air quality	April-18
36	Car Parking	Electric/hybrid vehicles incentives	Financial incentive for use of electric/hybrid vehicles	Improved local air quality	April-18

	Action	Sub-action	Action in detail	Desired outcome	Timescale
37	Car Parking	Reduced fees for low emission vehicles	Financial incentive for use of low emission vehicles	Improved local air quality	April-19
38	Business travel	Reduce car use for business travel	<ul style="list-style-type: none"> • Video conferencing to reduce the need to travel for meetings • Public transport (within 40 mins and with only 1 change) or cycling (2 miles) should be used for non clinical business travel. • Negotiate setting up a corporate scheme with TfGM to allow the Trust to sell Get me there cards. • Instead of staff using their own private cars, pool cars could be used for business travel. • Investigate if car club membership could be incorporated in non clinical business travel. 	Reduces the need for cars to be brought to site and lowers the CO ₂ emissions for business travel miles.	Ongoing
39	Motorcycle initiative	Increase motorcycle parking	Increase motorbike parking facilities on the site to support potential motor cyclists	Less cars and lower carbon footprint	May-15
40	Moped initiative	Moped lease scheme	Investigate the possibility of a moped lease scheme for those that are not on a public transport link or within cycle distance	Less cars and lower carbon footprint	May-16